



2025 Owner Representative Annual Reports

Prepared By:

MassDOT Highway Division

December 2025



2025 Highway Division Owner Representative Reports

- 1 606146 Canton- Norwood- Westwood- Dedham Street/I-95 Interchange
- 2 604173 Boston- Bridge Replacement, B-16-016, North Washington Street
- 3 606886 Montgomery- Russell- Bridge Preservation I-90 Over Us 20
- 4 604952 Lynn- Saugus- Bridge Replacement Belden G. Bly Bridge
- 5 606552 Northampton- I91 Bridge Replacements
- 6 605888 Taunton- Interchange Improvements At Routes 24 & 140
- 7 607977 Hopkinton- Westborough- Reconstruction Of I-90/I-495 Interchange
- 8 608049 Fall River- Corridor Improvements On Route 79/Davol Street
- 9 602659 Charlton- Oxford- Reconstruction On Route 20
- 10 607670 Boston- Bridge Rehab, B-16-067 (3gv), Maffa Way & Mystic Avenue
- 11 612231 Boston- Allston Viaduct Preservation
- 12 606783 Newton- Weston- At I-90/I-95 Bridge Bundle
- 13 606522 Andover- Lawrence- Bridge Rehabilitation I-495
- 14 613005 District 5- Off-System Bridge Bundle
- 15 605959 Boston- Chelsea- Tobin Bridge Preservation
- 16 612681 Boston- Tunnel Lighting Replacement On I-90 Eb & Wb (Crc 17h & Crc 17i)
- 17 604209 Holyoke- West Springfield- Rehabilitation Of Route 5
- 18 609346 Boston- Tunnel Lighting Replacement For I-93 (Nb/Sb) Ramps (Crc 17g)
- 19 605313 Natick- Bridge Replacement Route 27 Over Route 9
- 20 606496 Boston- Bridge Superstructure Replacement Bowker Overpass
- 21 609466 Haverhill- Methuen- Bridge Replacement I-495 Over Merrimack River I-495
- 22 605311 Marion- Wareham- Bridge Replacement, (Route 6) Over Weweantic River
- 23 606527 New Bedford- Bridge Replacements I-195
- 24 607887 Lowell- Rourke Bridge Replacement



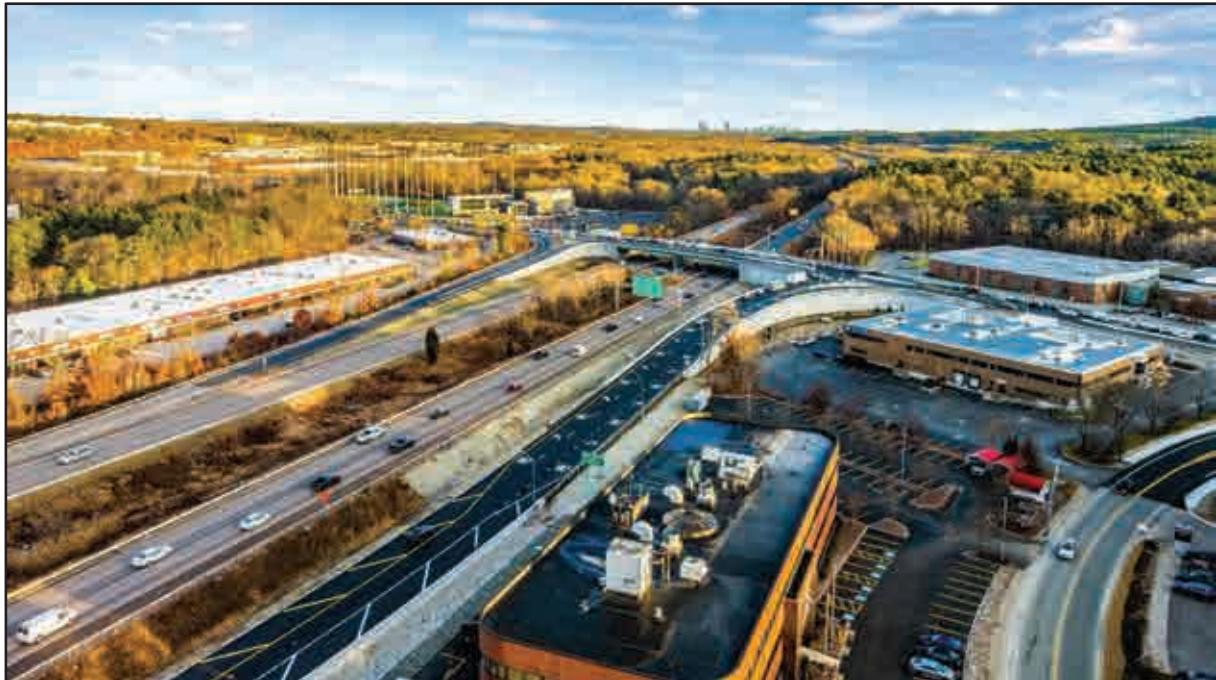
Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Ramp Construction on I-95 (NB) and
Improvements on Dedham Street/Canton Street

Owner's Representative Contract Number: 120632

Project Number: 606146
Construction Contract Number: 90726



Owner's Representative 2025 Annual Report

Owner's Representative Work Order No. 606146-3

***MassDOT Contract No. 90726: CANTON-NORWOOD-WESTWOOD - Ramp Construction
on I-95 (NB) & Improvements on Dedham Street/Canton Street***

Design Consultant: Stantec/AECOM

Contractor: SPS New England, Inc.

Prepared by Chappell Engineering Associates, LLC

December 1, 2025

Project Name: Ramp Construction on I-95 (NB) & Improvements on Dedham St./Canton St.

Owner's Representative Contract Number: 120632

MassDOT Project Number: 606146

Construction Contract Number: 90726

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I. EXECUTIVE SUMMARY

This project constructed a new off-ramp from I-95 northbound to Dedham Street in the Town of Canton and widened approximately 3800' of Dedham Street/Canton Street from two to four lanes in the Towns of Canton, Norwood and Westwood. The two-lane bridge over I-95 (1964) was replaced with a composite steel plate girder superstructure with reinforced concrete deck, 5-12' lanes comprised of two through-lanes in each direction and a left-turning lane for traffic from Dedham Street westbound to I-95 southbound. On Dedham Street the two-lane bridges over the Neponset River (2000) and over the MBTA/AMTRAK Railroad (1995) have both been widened to accommodate four travel lanes, minimum 5' shoulders, and a 6' sidewalk on the north side.

A construction contract in the amount of \$44,265,036 was awarded to SPS New England, Inc. in November 2016, and NTP issued on December 22, 2016. Full Beneficial Use was achieved on 12/1/23 and the facility fully opened to traffic. Substantial Completion has yet to be achieved as certain material certifications have yet to be provided by the Contractor. For a condensed history of the project from NTP through 2024 please consult prior years' Annual Reports; the present report is focused on events which occurred in 2025.

The last of the field activities completed in late 2024, and during the past year the Department and the Contractor have focused on completing the various tasks required to close out the contract. For the Contractor this has included submittal of five EWO requests encompassing eight PCOs which total to \$1,163,295. For the Department, this year to date a total of seven EWO requests encompassing fourteen PCOs have been reviewed and approved, totaling \$552,954. One EWO request dated 11/5/25, remains under review: PCO#70 for labor and materials escalation due to project delay in the amount of \$728,351. The Contractor has indicated that one final EWO request is under preparation, for extended overhead costs related to project delay beyond control of the contractor, and is expected to be submitted soon.

Three issues, discussed in detail in Section III, Cost Recovery, are currently being pursued for cost recovery. The two concerned design firms have been informed that they had been found responsible for additional costs due to design errors made by their firms. In two cases the firms have yet to respond; in the third case a meeting was requested with the Standing Committee which recently took place, on 12/3/25. The firm was directed to recalculate that portion of the total added cost for which they believe they are responsible and to return for further discussion in January.

The Department databases, in conjunction with the OR's projections, indicate that approximately an additional \$3.3 million will need to be added to the encumbrance monies to close out the project; please see the Budget and Project Financial section below. The District has requested an additional \$2.95 million from the FHWA to partially fund the overage and discussions are ongoing.

II. ANNUAL REPORT

Contract Scope of Work

The Dedham Street project added a new off-ramp from I-95 northbound to Dedham Street in Canton to complete the half-diamond interchange at this location (the southbound on-ramp was constructed in 2002). Dedham Street itself has been widened from two travel lanes to four travel lanes over a distance of approximately 3,800 feet. Five-foot shoulders are added in both directions to accommodate bicycles. Both the bridge over the Neponset River and the bridge over the AMTRAK/MBTA railroad tracks were widened to accommodate the added two lanes of traffic, five-foot shoulders and a northside sidewalk. A new, five-lane bridge carrying Dedham Street over I-95 was constructed, including an exclusive left-turn lane from Dedham Street westbound to the existing I-95 southbound on-ramp, a move not previously available. Coordinated traffic signal systems were installed at three locations along Dedham Street, a sidewalk was constructed the entire length of the corridor along the northern side of the roadway, and a new lighting system was provided along Dedham Street from east of the Neponset River across I-95 to Kirby Drive in Canton and along the new off-ramp. Dedham Street for much of the project alignment traverses existing wetlands associated with the Neponset River. In order to accommodate the increased roadway width without expanding the existing footprint, four MSE walls, three cast-in-place walls, and four stone masonry retaining walls were constructed to support the widened embankment. An additional three MSE walls were required along the two sides of the new NB off-ramp and one along the east side of the widened SB on-ramp.

Major Progress as of December 1, 2025

The Full Beneficial Use milestone was achieved on 12/1/23, and the facility fully opened to traffic. In addition to standard project close-out activities, in 2024 the contractor fabricated and installed 655 linear feet of snow fence at six locations where the height of the installed barrier did not comply with AASHTO guidelines; a portion of the cost for this work is being pursued as cost recovery (see Section III, Cost Recovery, below). As noted in the Executive Summary, during 2025 the Department and the Contractor have focused on completing the various tasks required to close out the contract. For the Contractor this has

Project Name: Ramp Construction on I-95 (NB) & Improvements on Dedham St./Canton St.

Owner's Representative Contract Number: 120632

MassDOT Project Number: 606146

Construction Contract Number: 90726

included submittal of five Extra Work Order (EWO) requests encompassing eight PCOs totaling \$1,163,295. For the Department, this year to date a total of seven EWO requests encompassing fourteen PCOs have been reviewed and approved, totaling \$552,954. One request, dated 11/5/25, remains under review: PCO#70 for labor and materials escalation due to project delay in the amount of \$728,351. The Contractor has indicated that a final EWO request is under preparation, for extended overhead costs related to project delay beyond control of the contractor, and is expected to be submitted soon. On the contract, to date forty-eight EWOs have been approved, totaling \$9,187,732.

Budget and Project Financials

A project cost summary follows:

• Bid Price:	\$44,265,036
• Original Encumbered Amount:	\$51,528,337
• Encumbrance Modifications:	\$4,827,581
• Current Encumbrance:	\$56,355,918
• Total Amount Invoiced as of December 1, 2025:	\$55,324,172

Breakdown by Encumbered Obligation and Allowance Items:

• SPS NE Bid:	\$44,265,036
• Contingencies:	\$4,436,617
• Traffic police:	\$2,318,085
• Railroad flaggers:	\$500,000
• Telephone:	\$3,000
• Trainees:	\$5,600
Total:	\$51,528,337

The table below contains a summary of project financial data as of December 1, 2025:

	Contract	MassDOT
1 Bid	\$ 44,285,038	\$ 44,285,038
2 Allowances	\$ 2,926,685	\$ 2,820,685
3 Original Contract Value	\$ 47,091,721	\$ 47,091,721
4 Original Contingency (FIN881)		\$ 4,436,617
5 Original Encumbrance		\$ 51,528,337
6 Encumbrance Modifications		\$ 4,827,981
7 Current Encumbrance		\$ 56,356,918
8 Current Overruns and Underruns	\$ (54,329)	\$ (54,329)
9 Approved Contract Modifications	\$ 9,358,009	\$ 9,358,009
10 Current Contract Value	\$ 56,902,059	\$ 56,902,059
11 Pending Contract Modifications	\$ 897,977	\$ 897,977
12 Estimated Contract Total	\$ 57,800,035	\$ 57,800,035
13 Probable Contract Modifications		\$ 1,728,351 Q
14 Forecast Cost-at-Completion	\$ 59,528,387	\$ 59,528,387
15 Total CQE (Invoiced) to Date	\$ 55,324,172	\$ 55,324,172
16 Funds Remaining		\$ 1,031,746
17 Projected Funds Remaining		\$ (3,172,469)

The “*Probable Contract Modifications*” line item, at \$1,728,351, is input by the Owner’s Representative and includes the Contractor’s EWO cost for materials and labor escalation currently under Department review and a reserve of \$1,000,000 for the expected upcoming EWO request for extended overhead. The “*Projected Funds Remaining*” currently stands at (\$3,172,469) and indicates that on the order of \$3.2 million in additional funds will be needed to complete the project. As noted previously, the District has requested an additional \$2.95 million from the FHWA to partially fund the overage and discussions are ongoing.

Schedule/ Project Milestones

FBU: 12/1/23

SC: Currently awaiting submittal of outstanding material certifications

Areas and Issues of Concern

Other than the need to identify additional funds to complete the project, at this time we have no significant areas or issues of concern.

Project Name: Ramp Construction on I-95 (NB) & Improvements on Dedham St./Canton St.

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Aerial Photos of the Completed Project



Looking north at Bridge. No. C-02-04, Dedham St. over I-95 with Exit 25 NB off-ramp



Project Name: Ramp Construction on I-95 (NB) & Improvements on Dedham St./Canton St.

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Looking southeast, Bridge No. C-02-04 Dedham St. over I-95 and the Dedham St. to I-95 SB on-ramp



Looking northwest, Bridge No. C-02-04 Dedham St. over I-95 and the Exit 25 I-95 NB off-ramp to Dedham St.

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

The peer review requirements for this project were satisfied during the design phase and continued during the construction phase. As construction is now complete peer review efforts are no longer required.

Value Engineering

A Value Engineering (VE) Study was conducted in 2013 by URS Corporation and Keville Enterprises. The study was conducted on the larger I-95/I-93 Interchange Improvement project; a study was not conducted for the Dedham Street widening and I-95 NB off-ramp as a separate project. The final report was submitted in January 2014.

Cost Recovery

To date three issues have been formally identified by the Cost Recovery Standing Committee as meriting continued investigation. The first is EWO #4 (\$47,592) concerning diaphragm modifications as discussed in the 1/6/21, 4/7/21 and 2/1/23 meetings of the Standing Committee; on each occasion the consensus was

that the EWO was cost-recoverable. EWO#4 resulted when it was determined during installation of electrical conduits under the Railroad Bridge that the two pier diaphragms needed to be re-fabricated due to interferences with the conduits. While two design firms participated in project design, AECOM and Stantec Consulting Services, Stantec is the designer of record of the Railroad Bridge. The Department initially notified Stantec on 11/5/20 that a cost recovery procedure was being initiated; they responded by letter on 11/18/20 that the root cause of the EWO was a post-bid reduction by NGRID in the number of allowable conduit bends between pullboxes and thus was not due to “designer error”. By letter dated 7/10/23 the Department informed Stantec that they were liable for the full cost of EWO #4 and offered three potential courses of action: 1) make a direct payment to MassDOT for the requested amount; 2) provide consulting services of equal value at no cost to the Department under an existing contract; or 3) schedule a meeting to present additional information. To our knowledge the design firm has yet to respond.

Background on EWO#s 31 and 40 is as follows. During the final push to open the roadways, in late November 2023, District personnel noted that the barrier height at the retaining walls on the south side of Dedham St. was 32” measured from the roadway surface whereas, due to the presence of the adjacent, eastbound bike lane, AASHTO recommends a minimum height of 42”. The Contractor was directed to provide a temporary wooden handrail prior to the December 1 roadway opening, with a permanent solution to follow. EWO #31 (\$11,473) for temporary wooden handrail was approved in October 2024, and EWO #40 (\$640,730) for 655 linear feet of 36” snow fence at four locations was approved in September 2024. Both design firms were responsible for portions of the walls in question, AECOM (157 of the 655 linear feet, 24%) and Stantec Consulting (498 linear feet, 76%). Both EWOs were discussed during the 11/6/24 meeting of the Cost Recovery Standing Committee with the consensus being that continued cost recovery efforts were merited in both cases. The Designers maintain that the lack of any handrail at the locations was an “item omission” and not cost-recoverable. Similarly, they maintain that the decision to install snow fence as opposed to the standard 1-bar steel railing as recommended represents a “betterment” and any increase in cost is not cost-recoverable. In the November 2024 meeting the Standing Committee requested that Assistant District Construction Engineer determine that portion of the EWOs which were neither an item omission nor a betterment. The cost-recoverable amounts were subsequently recalculated and 10/15/25 letters were issued to each firm. AECOM was assessed the sum of \$106,239, 24% of the cost-recoverable amount from the two EWOs, and Stantec Consulting the sum of \$336,425. The letters offered three potential courses of action to the design firms: 1) make a direct payment to MassDOT for the requested amount; 2) provide consulting services of equal value at no cost to the Department under an existing contract; or 3) schedule a meeting to present additional information. Stantec requested a meeting which occurred on

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12/3/25. They presented arguments as to why the assessed amount appeared excessive, and the Committee directed that they recalculate what they believe to be fair amount and return to discuss at the Committee's January's meeting.

Initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due as the formal evaluation process outlined in SOP No. CSD 25-14-1-000 (11/11/22) must be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for referral to the Cost Recovery Standing Committee.

Oath

I, Richard B. Littlefield P.E., MA P.E. No. 38773, hereby certify that my sole responsibility as Owner's Representative, under OR Contract No. 120632, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.



Richard B. Littlefield, P.E.

Date: December 1, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: North Washington Street Bridge Project
Owner's Representative Contract Number: 120635
Project Number: 604173
Construction Contract Number: 102269



Owner's Representative's 2025 Annual Report

Owner's Representative Contract No.: 120635 JRP Consulting Services, LLC

Owner's Representative Work Order No.: 1

Project No. 604173 BOSTON-BRIDGE REHABILITATION, B-16-016, North Washington Street Over Boston Inner Harbor

Project Designer: Alfred Benesch & Company

Contractor: JF White Contracting Company

Prepared by: Jeffrey Paul, P.E., DBIA

December 16, 2025

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I EXECUTIVE SUMMARY

The North Washington Street Bridge was a historic structure constructed in 1898. The bridge consisted of 10 approach spans and a swing span, which was not operational. The bridge was deemed structurally deficient and was posted for restricted loads. There had been extensive emergency repairs done to the bridge over the past decade which led to closure of the two center lanes on the swing span due to steel deterioration. The City of Boston Public Works Department proposed to replace the bridge and retained Alfred Benesch & Company to provide engineering services required to prepare construction documents for the bridge replacement. MassDOT is responsible for project advertisement, administering Federal funds associated with the project, and construction oversight.

The work under this project consisted of the design and construction of the replacement of the twelve (12) span, 1087' long bridge with an 'iconic' 1100' long by 101.5' wide bridge that included four (4) lanes of traffic, two (2) in each direction, one (1) dedicated bus lane, dedicated bicycle lanes, pedestrian walkways, an architectural trellis structure on each side of the bridge, full length planters along the barrier for each side of the bridge and trees. Project goals included: replacement of structurally deficient bridge, incorporation of new multi-modal bridge as a complete City street over water, improvements to deficient bike and pedestrian accommodations, improvements to intersections safety and functionality, improvements to navigational channel, design of context sensitive bridge in important waterfront and historic bridge location, creation of a visually appealing bridge design that complements the Zakim Bridge.

The construction contract was awarded to JF White Contracting Company (JFW) in April 2018. In May 2018, JFW submitted a full length temporary vehicular bridge value engineering cost proposal (VECP) as a schedule reduction measure. MassDOT evaluated the merits of the proposal and provided JFW with approval to proceed provided all necessary permits could be secured by JFW. In May 2019 all permits had been approved by the respective permitting agencies and construction of a temporary utility bridge and a combined pedestrian and vehicular bridge commenced. Vehicular and pedestrian traffic were diverted onto the temporary bridge on July 17, 2020 and remained in this configuration until December 9, 2023 when vehicular and pedestrian traffic shifted onto the new permanent bridge.

MassDOT and JFW have committed to keeping the public informed as well as managing impacts as best they can while getting the project completed. On-going public outreach and engagement efforts included bi-weekly updates on the project website, two-week construction look-ahead emails, traffic advisories, media outlet outreach, social media, and public information meetings.

In October 2021 systemic cracks were found in complete joint penetration (CJP) welds at internal diaphragms in fabricated North Washington Street Bridge tub box girders (located both at Casco Bay Steel facility and within those installed in the field). JFW was subsequently directed to cease all contract work associated with tub girders until a root cause analysis was performed and subsequent corrective action measures were developed and approved. Upon approval of a corrective action plan, field activities to repair weld cracks in tub girders, that were located at both the Casco Bay facility and at the project site, commenced in September 2022. On January 13, 2023, field activities to repair weld cracks in tub girders, located at both the Casco Bay facility and the project site, were completed. JFW filed Claim No. 6-102269-003 and put MassDOT on notice that they will have incurred cost attributable to the weld crack issue and associated delay in the amount of \$16.3M. On December 12, 2022, MassDOT denied JFW claim No. 6-102269-003 and maintained the position that weld cracking was a direct result of the Contractor's fabrication process and lack of sufficient quality control. As of November 30, 2025, a determination as to culpability has yet to be decided.

As of November 30, 2025, construction is approximately 97% complete. The original contract duration was approximately 5.25 years with a contractual substantial completion date of August 9, 2023. Substantial completion was achieved on October 30, 2025, or -813 days behind schedule. The total project encumbrance is \$204,393,098 of which \$182,295,272 relates to MassDOT work and \$22,097,826 relates to Municipal work. Excluding the potential cost of \$16.2M associated with the weld crack issue, the project is currently within budget. On September 24, 2025, there was a ribbon cutting celebration of the William Felton "Bill" Russell Bridge.

II ANNUAL REPORT

Contract Scope of Work

Scope of work for the North Washington Street Bridge Replacement project consisted of the replacement of the existing twelve (12) span, 1087' long bridge with an 'iconic' 1100' long 11-span continuous trapezoidal steel box girder bridge that included four (4) lanes of traffic, two (2) in each direction, introduction of an exclusive bus lane (the first on a bridge in the Boston area) southbound from North Washington Street/Rutherford Avenue intersection with Chelsea Street, southbound over the bridge, and ending prior to the North Washington Street intersection with Causeway Street, dedicated (one direction) separated bicycle lanes on both sides of the bridge, pedestrian walkways on both sides of the bridge including a scenic overlook and seating area, full length planters along the barrier for each side of the bridge and trees, increasing intersection safety and capability, and improving the navigational channel. An architectural trellis will enhance these destination points for pedestrians and aesthetic ornamental lighting will be added above and below the bridge.

Major Progress As Of November 30, 2025

Major project elements completed in 2025 included:

- JFW maintained noise monitoring and pest control programs
- JFW conducted ongoing meetings with project abutters and stakeholders
- JFW held weekly progress meetings every Wednesday. Topics discussed included safety, community, quality control, schedule, RFIs, submittals, engineering and technical matters, utilities, and open items
- JFW completed main portion of bridge superstructure including bridge deck, roadway barriers, planters, benches, light poles, and sidewalks
- JFW completed FRP panel installation
- JFW completed installation of stainless-steel handrails
- JFW completed trellis lighting
- JFW completed Lovejoy Walkway

- JFW completed full depth construction at Keany and City Squares
- JFW completed below water fender system
- JFW completed installation of dolphin piles
- JFW completed DCR Restoration Area
- JFW completed plantings and irrigation system
- JFW painted exterior and interior tub girders

Budget

This project is funded through the 2016 Transportation Improvement Program (TIP) for the Metropolitan Planning Organization. A project cost summary follows:

• Office Estimate:	\$174,388,203
• Bid Price:	\$176,777,389
• Encumbered Amount (MassDOT):	\$182,295,272
• Municipal Contract Value:	\$ 22,097,826
• Total Encumbered Amount (MassDOT & Municipal):	\$204,393,098
• Forecast Cost-at-completion:	\$213,689,874*
• Anticipated Budget (Over)/Underrun-at-completion:	(\$8,451,566)
• Total Amount Expended through November 30, 2025:	\$184,939,144

* Includes potential cost of \$16.2M associated with the tub girder weld crack issue. When excluding the cost of \$16.2M, the project is currently within budget.

Total Encumbered Obligation by Appropriation (MassDOT):

• 61211714 Federal Participating	NHP(BR-ON)-0035(054)	\$180,538,119
• 61211317 Non-Federal Aid	100% State	<u>\$ 1,750,000</u>
	Total	\$182,288,119

Breakdown by Encumbered Obligation and Allowance Items (MassDOT & Municipal):

• JF White Bid:	\$176,777,389
• Contingencies:	\$ 16,187,956
• Encumbrance Modifications:	\$ 7,153
• Incentives (Municipal):	\$ 7,200,000

• Telephone:	\$ 15,000
• Traffic Police:	\$ 4,200,000
• Trainees:	\$ 5,600
Total	\$204,393,098

The table below contains a summary of project financials through November 30, 2025.

Project Budget / Financials			
	Contract	MassDOT	Modified
① Bid	\$ 176,777,300	\$ 161,879,553	\$ 142,897,626
② Allowances	\$ 11,420,601	\$ 4,220,888	\$ 2,209,000
③ Original Contract Value	\$ 188,197,901	\$ 166,100,141	\$ 145,097,626
④ Original Contingency (FMW635)		\$ 16,107,951	
⑤ Original Encumbrance		\$ 102,288,119	
⑥ Encumbrance Modifications		\$ 7,151	
⑦ Current Encumbrance		\$ 102,295,271	
⑧ Current Overruns and Underruns	\$ (1,786,325)	\$ (1,776,020)	\$ (10,955)
⑨ Approved Contract Modifications	\$ 8,215,376	\$ 8,214,958	\$ 200,917
⑩ Current Contract Value	\$ 195,647,050	\$ 174,130,202	\$ 145,080,838
⑪ Pending Contract Modifications	\$ 541,023	\$ 206,823	\$ 634,169
⑫ Estimated Contract Total	\$ 196,788,073	\$ 173,335,025	\$ 205,714,038
⑬ Probable Contract Modifications		\$ 16,001,813	
⑭ Forecast Cost at Completion	\$ 213,659,674	\$ 190,746,837	\$ 225,431,038
⑮ Total COT (Download to Date)	\$ 184,939,144	\$ 166,034,308	\$ 15,104,838
⑯ Funds Remaining		\$ (2,643,272)	
⑰ Projected Funds Remaining		\$ (8,451,366)	
⑱ Expected Progress (baseline)	100.00%		
⑲ % Complete	95.55%		

Schedule/ Project Milestones

The latest Monthly Progress Schedule available is Update No. 80 which on November 20, 2025, was accepted as noted by MassDOT.

The contractual Milestone 1 (Contractor Field Completion) date for the North Washington Street Bridge Replacement Project is November 7, 2023. The forecasted Milestone 1 (Contractor Field Completion) date contained in the November 1, 2025 Monthly Progress Schedule is June 21, 2026 which is trending -957 calendar days behind the contractual Milestone 1 date.

The contractual Milestone 2 (Substantial Completion) date is August 9, 2023. JFW declared that Milestone 2 was achieved on October 30, 2025 which was -781 calendar days behind schedule.

The November 1, 2025 Monthly Progress Schedule is forecasting JFW to miss the Milestone 2 Incentive Payment date by -987 calendar days, February 16, 2023 versus October 30, 2025. As a result, this may be subject to a disincentive payment.

The contractual Milestone 3 (Full Beneficial Use) date is May 1, 2023. JFW declared that Milestone 3 was achieved on June 30, 2025 which was -781 calendar days behind schedule.

The following Table, from Monthly Progress Schedule Review (Update 80) as prepared by Keville Enterprises, identifies a summary of Critical Activities.

Additional Critical Activities	Baseline Date	1-Nov-25 Update 80	Days Ahead or Behind
Temporary Utility Bridge Structure - Completed	14-Jun-2019	16-Oct-2019	-124
Temporary 24" National Grid Gas Line - Completed	17-Jun-2019	10-Dec-2019	-176
Temporary Vehicular Bridge - Open to Traffic - Completed	23-Nov-2019	18-Jul-2020	-238
Close / Start Demolition - Existing North Washington Street - Completed	25-Nov-2019	18-Jul-2020	-236
Proposed North Washington Street - Open to Traffic - Partial - Completed	13-Dec-2021	9-Dec-2023	-726
Proposed North Washington Street - Open to Traffic - Full - Completed	24-Oct-2022	20-Jun-2025	-970
MS 3 - Full Beneficial Use (Incentive Milestone) - Completed	16-Feb-2023	20-Jun-2025	-855
MS 3 - Full Beneficial Use (Contractual) - Completed	1-May-2023	20-Jun-2025	-781
MS 2 - Substantial Completion (Incentive Milestone) - Completed	16-Feb-2023	30-Oct-2025	-987
MS 2 - Substantial Completion (Contractual) - Completed	9-Aug-2023	30-Oct-2025	-813
MS 1 - Contractor Field Completion (Incentive Milestone)	17-May-2023	21-Jun-2026	-1131
MS 1 - Contractor Field Completion (Contractual)	7-Nov-2023	21-Jun-2026	-957

Contract Milestones:

Milestone 1 – Final Acceptance: The Contractor shall achieve Contractor Field Completion within 1,917 Calendar Days after notice to Proceed (NTP).

Milestone 2 – Substantial Completion: The Contractor shall achieve Substantial Completion within 1,827 Calendar Days after Notice to Proceed (NTP).

Milestone 3 – Full Beneficial Use: The Contractor shall achieve Full Beneficial Use within 1,727 Calendar Days after Notice to Proceed (NTP).

Critical Path Discussion

The Critical Path is driven by the completion of painting steel box beam tub girders. The Critical Path travels through the remaining punch list activities to attain Contractor Field Completion.

Areas and Issues of Concern

There are currently no areas of concern regarding projected cost, however, MassDOT has potential risk to financial exposure if the root cause of the tub girder diaphragm weld crack issue is determined to have been a result of the requirements contained within the contract documents. As of November 30, 2025 a determination as to culpability has yet to be determined. JFW has informed MassDOT that they will have incurred cost attributable to the weld crack issue and associated delay in the amount of \$16,292,827.23. The project team developed an acceptable corrective action plan and as of January 13, 2023 all weld crack repairs at both the Casco Bay facility and project site have been completed. On February 1, 2023, MassDOT approved JFW proposal to fabricate a new tub girder 5G1B, however, MassDOT reiterated their position that all costs associated with fabrication of the new tub girder are the responsibility of JFW as these costs are attributed to the Internal Diaphragms Weld Cracks issue which are a direct result of the JFW's fabrication processes and lack of sufficient quality control protocols. JFW indicated that the cost to fabricate a new tub girder is \$385,000 (this cost is inclusive of the \$16,292,827.23 cost noted above). As of November 30, 2025 JFW has installed all remaining tub girders.

As of November 30, 2025 the contract is projected to be completed below the encumbered amount with no additional funds needed, however, should MassDOT be determined to be culpable for weld cracks and have to pay JFW \$16,292,827.23 (as submitted by JFW), then an additional \$8,451,566 would be needed. Although this OR believes that weld cracks were a direct result of fabrication issues, the maximum known exposure, at this time, is being carried as a potential contract modification. On December 12, 2022, MassDOT denied JFW claim No. 6-102269-003 and maintained the position that weld cracking was a direct result of the Contractor's fabrication process and lack of sufficient quality control. As of November 30, 2025 MassDOT is in negotiations with JFW to settle all open issues, including the weld

crack matter.

The following list represents potential areas of concern identified for the project that may impact the project budget and/or schedule:

- Cracks found in the welds connecting the end diaphragms to the girder webs in tub girders, first identified in the September 2021 Monthly Report, remains an ongoing concern that has the potential to adversely affect the project schedule. Through Deficiency Report (DR) No. 001, issued on October 1, 2021, JFW was advised that no further work may proceed that would in any way obstruct their ability to correct this deficiency or the Department's ability to inspect the area until the deficiency has been corrected and approved by the Department. JFW was also advised that any associated costs to the Department related to this deficiency may be back-charged to JFW. Furthermore, on October 27, 2021 MassDOT issued formal direction to JFW to cease all work activities on the North Washington Street bridge structure to include ceasing the installation of the NGRID Gas line, installation of the SIP deck pans, and any and all bolting that may add additional loading to the superstructure and the tub girders. JFW was also directed to execute and carry out any and all measures to make the bridge structure safe and secure. JFW was informed that all other Contract work outside of the bridge, warehouse span repairs, and work to close the slip lane to Chelsea Street may continue to proceed. On October 28, 2021 MassDOT directed JFW to conduct a thorough engineering analysis to determine the root cause(s) that resulted in vertical cracking in welds encountered in the tub girders at the Casco Bay fabrication plant and on the project site. JFW provided the results of their root cause analysis to MassDOT on March 10, 2022, which included an opinion from SGH. Additionally, on March 25, 2022 JFW's steel fabricator, Casco Bay, and Casco Bay's consultant WJE, issued their own root cause analysis. On April 4, 2022 JFW issued Letter No. 216 regarding Internal Diaphragm Weld Cracks Draft Testing, Mapping, and Corrective Action Procedures by Casco Bay Steel Structure's (CBSS). On April 19, 2022 MassDOT issued response to JFW Letter NO. 216 regarding Internal Diaphragm Weld Cracks Draft Testing and Corrective Action Plan submitted by JFW. MassDOT noted that they reviewed JFW Letter No. 216 and subsequently request JFW to provide responses to the questions/comments contained in IOM from MassDOT's Bridge Group Boston Construction Engineering Department. MassDOT

Bridge Group noted that JFW's corrective procedure to only address cracks identified to date is not acceptable and that JFW should revise the corrective action procedure to including the Heat Affected Zone (HAZ) of the CJP welds, regardless of whether a crack has formed at this time, to address the increased hardness and embrittled microstructure. On May 19, 2022 a Step 1 Claims meeting was held by MassDOT. On June 24, 2022 JFW provided MassDOT with an itemized statement of claim noting that through May 31, 2022 they have incurred \$2,841,473.54 of costs attributable to the weld crack issue and associated delay. JFW also notes time impacts of 221 days of delay. In August 2022 JFW and Casco Bay performed 2 independent weld crack repair mock-ups, both producing favorable results. As of March 31, 2023 this matter remains on-going, however, field activities to repair weld cracks in tub girders that are located at both Casco Bay and at the project site have been completed. On March 15, 2023 JFW requested a Step 2 hearing with MassDOT's Claims Committee. On September 27, 2023 MassDOT's Owner Representative recommended that talks be initiated regarding inspection of tub box girders prior to the opening of the bridge to the general public, now that the tub girders have experienced substantial dead load, as well as live loads with construction vehicles. MassDOT concurred and a meeting with MassDOT Bridge Group, HNTB, Benesch, and technical experts was held October 4, 2023, at which time it was mutually agreed that HRV would perform visual inspections of all vertical welds, MT all full depth diaphragms, and spot check partial depth diaphragms, of all constructed tub girders. On November 3, 2023 HRV completed their inspection and noted the presence of one small crack (1/2" long). It was determined that this crack was likely missed during the previous rounds of UT testing and repairs. As of November 30, 2025 the crack was repaired in accordance with previously approved procedures.

- NGRID original Force Account agreement budget exceeded. Phase I of the NGRID's gas scope included the installation of the 24" temporary gas line on the temporary utility bridge for the NWS Replacement Project. Actual costs for the temporary gas line exceeded NGRID's original estimate by approximately \$1.1 million. As a result, MassDOT may require additional funding to complete NGRID Force Account agreement scope as the permanent gas line work has not been completed. It should be noted that the original design did not include scope for a temporary vehicular bridge as proposed, designed, and constructed by JFW. The incorporation of JFW's VECP temporary bridge resulted in NGRID incurring time delays and additional

mobilizations which were not memorialized in their original force account agreement with MassDOT. MassDOT may seek reimbursement from JFW to help mitigate the additional cost incurred by NGRID and on January 31, 2022 MassDOT informed FHWA additional funding would be required. On May 25, 2022 MassDOT reminded FHWA that additional funds will be required. On September 27, 2023 MassDOT requested payment in the amount \$1,120,061 for the remaining payment for NGRID Phase I work. On October 18, 2023 MassDOT recommended that partial payment, in the amount of \$851,000, be issued to NGRID with additional funds of approximately \$268,000 be withheld until NGRID provides further justification of overhead costs. On May 1, 2024 MassDOT requested that NGRID issue a partial payment invoice to close out this phase of the project. On June 24, 2024 NGRID submitted a partial payment invoice in the amount of \$3,238,551.39 bringing total amount invoiced, through June 15, 2024, equal to \$5,523,136.63 (Total amount paid in Phase I = \$2,284,585.24). As the total amount on the original agreement was \$3,229,017.21, As a result of the overrun, \$2,294,119.42 additional funding will be needed for the NGRID force account agreement. In April 2025, NGRID provided MassDOT with Phase II Cost in the amount of \$3,516,625.28 which was an increase from their original Phase II cost estimate of \$1,825,815.64. In August 2025 NGRID issued a revised cost estimate in the amount of \$3,434,767.40. As of November 30, 2025, District 6 Construction has requested a lump sum agreement in the amount of \$3,434,767.40 for NGRID. Approval is pending.

Project Name: North Washington Street Bridge Project
Owner's Representative Contract Number: 120635
Project Number: 604173
Construction Contract Number: 102269

Project Photographs



Project Location



Historic North Washington Street Bridge

Project Name: North Washington Street Bridge Project
Owner's Representative Contract Number: 120635
Project Number: 604173
Construction Contract Number: 102269



Proposed Bridge Rendering



Aerial View January 2025



Aerial View February 2025

Project Name: North Washington Street Bridge Project
Owner's Representative Contract Number: 120635
Project Number: 604173
Construction Contract Number: 102269



Aerial View March 2025



Aerial View April 2025



Aerial View May 2025



Aerial View June 2025



Aerial View July 2025



Aerial View August 2025



Aerial View September 2025



Aerial View October 2025



Aerial View November 2025



North Washington Street Bridge

III CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

Formal peer reviews of design submissions, including structural, highway, environmental, geotechnical, traffic, utility and constructability were performed for MassDOT by HNTB on October 31, 2016, and May 27, 2017.

During the construction phase of this project, this O.R. will satisfy peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon

any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

Value Engineering

In June 2015 MassDOT commissioned Jacobs Engineering Group Inc. (Jacobs) to perform a 5-day Value Engineering study for the North Washington Street Bridge Replacement Project. The Value Engineering (VE) Team included representatives from Jacobs, Huie Construction Services, Green International and this OR. The VE Team focused on opportunities for value improvement including, but not limited to project alternatives and respective costs, maintaining traffic flow, marine work environment and foundation options, optimization of construction schedule and staging, bridge aesthetics, and accommodations to pedestrians, bicycles, and the Freedom Trail.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues. However, it should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss referral to the Cost Recovery Standing Committee. For any issue referred for Cost Recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 8/7/17) shall govern. In order to perform the legislative mandate of being the Primary Manager of Cost Recovery, the Owner's Representative will follow the referenced SOP. This SOP, as modified for projects with an Owner's Representative, describes the role of the Owner's Representative and includes a Cost Recovery Standing Committee. This committee oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary.

Approved/Pending/Probable Extra Work Orders (EWO's)

As of November 30, 2025, there have been twenty-four (24) Approved Contract Modifications.

TOTAL APPROVED CONTRACT MODIFICATIONS = \$4,158,368.60

As of November 30, 2025, there are three (3) pending Contract Modification.

TOTAL PENDING CONTRACT MODIFICATIONS = \$634,198.65

As of November 30, 2025, there are four (4) probable EWOs. These include:

1. EWO XX – Warehouse Elastomeric Bearing Pads
 - a. Disposition: approval is pending
 - b. Cost: \$2,443.20
2. EWO XX – Permanent Traffic Signal Changes
 - a. Disposition: approval is pending
 - b. Cost: \$4,173.26
3. EWO XX – Permanent FRP Shielding Bolts
 - a. Disposition: approval is pending
 - b. Cost: \$72,555.82
4. EWO XX – Tub Girder Weld Cracks *
 - a. Disposition: determination TBD
 - b. Cost: \$16,292,827.23

* As of December 12, 2022, MassDOT has denied JFW Claim No. 6-102269-003

TOTAL PROBABLE CONTRACT MODIFICATIONS = \$16,374,999.51

Project Name: North Washington Street Bridge Project
Owner's Representative Contract Number: 120635
Project Number: 604173
Construction Contract Number: 102269

Owner's Representative Oath

I, Jeffrey R. Paul, P.E., MA P.E. No. 38526, hereby certify that my sole responsibility as Owner's Representative, under OR Contract 120635, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

A handwritten signature in blue ink that reads "Jeffrey R. Paul".

Jeffrey R. Paul, P.E., DBIA

Date: December 16, 2025

Owner's Representative



Mauro Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Montgomery-Russell
Westfield River Bridge
Project No. 606866
Contract No. 108890



Owner's Representative's 2025 Annual Report

OR Contract No.: 120631 Fuss & O'Neill, Inc.

***Project No. 606886 MONTGOMERY-RUSSELL –Bridge Rehabilitation
Westfield River Bridge –I-90 Over U.S.-20, Westfield River, and CSX Project***

Contractor: Daniel O'Connell Sons, Inc.

Designer: Parsons Transportation Group

Prepared by: Fuss & O'Neill, Inc.

December 16, 2025

MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,

Westfield River, and CSX

Project No. 606866, Contract No. 10889

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EXECUTIVE SUMMARY

Bridge M-30-008=R-13-018 is an eight-span bridge carrying two lanes of I-90 Eastbound and two lanes of I-90 Westbound over US Route 20, the Westfield River, a CSX Railroad track, and over other terrain, between the Towns of Montgomery and Russell. Originally constructed in 1957, a deck replacement and minor repairs project was completed in 1990. In 2010-2011 the truss gusset plates and top flanges of some of the cantilever brackets were repaired and some portions of the bridge were repainted under a rehabilitation contract.

The superstructure in spans 1 and 2 consists of ten continuous rolled steel beams. The superstructure in spans 3, 4, 5 and 8 consists of girder-floor beam-stringer system. The superstructure in spans 6 and 7 consists of a continuous steel deck truss with built up riveted box section as truss members. The substructure consists of two solid stem reinforced concrete piers at piers 1 and 2, two reinforced concrete columns with a reinforced concrete cap at piers 3 and 4, three solid stem reinforced concrete piers with columns at piers 5, 6, and 7; and two reinforced concrete stub abutments.

The contractor had been rehabilitating the concrete and steel substructure since work began in 2020. As part of the contract, no work was to begin on the deck, barriers and roadway until all of the steel work below has been completed. This work to rehabilitate/replace steel components of the truss substructure was essentially completed in April 2023. After that the deck and other topside components work began and is tracking to be completed in the fall of 2026.

The designer of the project was Parsons. The project was awarded to Daniel O'Connell's Sons on March 23, 2020.

ANNUAL REPORT

Contract Scope of Work:

This contract consists of the rehabilitation of the bridge and associated roadway work to restore the roadway to existing conditions after traffic modifications are no longer necessary. The bridge will be rehabilitated in multiple stages to facilitate traffic flow at full capacity during the entire project. Essentially the project consists of substructure concrete repairs at piers and abutments, steel repairs, replacement, and painting, changing out rivets to high strength bolts, cleaning and painting structural steel, repairs to or replacing the deck concrete, adding highway lighting, protective screening, drainage improvements, upgrading to single faced outside barriers meeting current standards, and adding a new median barrier.

Major Progress as of November 30, 2025

In January and part of February of 2025, Stage IV deck and barrier work was completed. Stage IV involved construction in a 20+ foot wide area of the north side of the bridge, while westbound traffic was shifted towards the middle of the bridge. Traffic remained in this pattern while minor deck repairs were made and the expansion joints were placed while waiting for the more temperate weather of April to make it feasible to spray the membrane for the area and a surface pavement course could be placed. Work in this stage also included protective safety screen erection and highway lighting placement atop the fascia barrier.

In May, Stage V traffic pattern was put into place with temporary barrier and new lane striping. This stage involved westbound vehicles traveling on either side of an 11 foot work area in order for deck repairs to be completed within the area. This stage had a short duration because it involved no barrier excavation or placement. It was completed in June and Stage VI was begun.

MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,

Westfield River, and CSX

Project No. 606866, Contract No. 10889

Stage VI involves the median area of the bridge with both eastbound and westbound traffic crowded towards their respective fascia barrier. Work consists of deck repairs, cast in place median barrier construction, expansion joints, membrane placement and paving in addition to new median barrier off the bridge at each approach. This Stage is expected to last until May 15, 2026 when Full Beneficial Use is currently scheduled for.

Regarding the substructure for 2025, spalled concrete repairs have continued throughout, three coat painting system for the steel continued, areas of pack rust were addressed, and shim pack repairs were completed. Finally, the lower Safespan work platform was removed.

Budget:

Bid Price: \$46,973,440

Estimated Contract Cost: \$49,545,475

Encumbered* Amount: \$55,291,599

*Includes Contingencies, Railroad Flaggers, Traffic Police, Specialty Services, and Test Pit Excavations.

Total Amount Expended** through Nov. 1, 2025: \$52,843,134.

Percent Completion to Date: 91.00

Current Estimate at Completion: \$58,070,100.

Schedule:

The Contractor was issued a Notice to Proceed on March 23, 2020. The Substantial Complete date was set for November 25, 2024 but was extended to November 3, 2025 for inclusion of fabrication and installation of 6 foot high barrier fencing to be placed on top of the exterior barriers for the entire length of the structure instead of just over the railroad.

MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,

Westfield River, and CSX

Project No. 606866, Contract No. 10889

The contractor maintains that having to mill, place membrane, and pave at the end of each Stage of the construction versus just doing it all at once at the end of the project as they had planned has set the project back. Now they are forecasting Substantial Completion to be August 27, 2026. The Contractor was granted an extension of 145 days and another 140 day extension is being reviewed and deemed to have merit as part of PTEA07. Part of this extension request is due to much higher amount of substructure concrete repairs actually being worked on than originally calculated. These repairs involve extension access in order to be completed.

Areas and Issues of Concern

Financially, quantity overruns such as reinforced concrete excavation, the three coat painting system, the substructure concrete repairs, plus the addition of the 6 foot high barrier fencing being added to the project, the Current Estimate at Completion stands at \$58,070,100 while the Encumbered Amount was \$55,292,599 leading to request a likely add funds amount of \$2.8M. As the project nears completion, the budget will have to be closely monitored and additional funds will need to be added to the contract.

**MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,
Westfield River, and CSX**

Project No. 606866, Contract No. 10889

Project Photographs



Finished Pavement at Stage IV Construction Area

**MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,
Westfield River, and CSX**
Project No. 606866, Contract No. 10889



Full Depth Excavation at Expansion Joint – Stage 5 Construction

**MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,
Westfield River, and CSX**
Project No. 606866, Contract No. 10889



Staging for Pier 7 Spalled Concrete Substructure Repair

**MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,
Westfield River, and CSX**
Project No. 606866, Contract No. 10889



Final Polymer Modified Tack Coat and Membrane Stage V

**MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,
Westfield River, and CSX**
Project No. 606866, Contract No. 10889



Placed Concrete at Center Median Barriers Stage VI

**MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,
Westfield River, and CSX**
Project No. 606866, Contract No. 10889



Form Work for Median Barrier Sections

CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

As part of the initial design phase for this project, evidence supports that a peer review was performed at each stage of the design by MassDOT's Highway Division forces independent of the project designer. As the design process developed, Addenda was issued, primarily involving better defining the final product desired. These reviews were performed to identify conflicts or errors that should be corrected or mitigated at each stage before the design progressed to the next stage.

During the construction phase of this project, this O.R. is satisfying the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

Value Engineering

It is the Owner's Representative's understanding that no Value Engineering process was utilized on this Project.

Cost Recovery

There have been overruns of quantities relating primarily to initial access issues and another being additional quantity of an item that was brought to light and added during the Addendum process but never carried into the contract documents.

Another overrun has come to light during the painting process. The contract documents reflect a quantity of 46,000 sf with no calculations to substantiate the quantity. The final quantity is actually nearing 85,000 sf and leading to an overrun of nearly 39,000 sf at a cost of \$3,802,960. Because this item will end up more than 25% over the original quantity, MassDOT is currently asking for this item price to be renegotiated.

However large these overruns are, it does not appear that these rise to the level of errors leading to cost recovery with conventional industry thinking being that if these quantities were included in the original contract, the cost to the contract would have been that much more at bid time.

MONTGOMERY-RUSSELL –Bridge Rehabilitation Westfield River Bridge –I-90 Over U.S.-20,

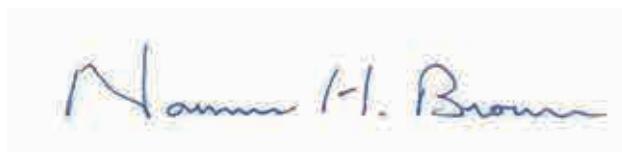
Westfield River, and CSX

Project No. 606866, Contract No. 10889

It should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for its referral to the Cost Recovery Standing Committee. For any issue referred for Cost Recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 8/7/17) shall govern. In order to perform the legislative mandate of being the Primary Manager of Cost Recovery, the Owner's Representative will follow the referenced SOP. This SOP, as modified for projects with an Owner's Representative, describes the role of the Owner's Representative and includes a Cost Recovery Standing Committee. This committee oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary.

OATH

I, Norman H. Brown, P.E., MA P.E. No. 36531, hereby certify that my sole responsibility as Owner's Representative, under OR Contract 80661, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

A handwritten signature in blue ink that reads "Norman H. Brown". The signature is fluid and cursive, with "Norman" in blue and "H. Brown" in blue.

Norman H. Brown, P.E., P.L.S.

Date: December 16, 2025



Mass. H. Baker, Governor
K. Driscoll, Lieutenant Governor
Philip E. Erb, Interim MassDOT Secretary
Jonathan L. Faillace, Undersecretary and Highway Administrator



Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

Owner's Representative Contract Number: 120632

MassDOT Project Number: 604952

Construction Contract Number: 109691



Owner's Representative 2025 Annual Report

Owner's Representative Contract No. 120632 - Chappell Engineering Associates, LLC
Owner's Representative Work Order No. 604952-4

MassDOT Contract No.109691: LYNN – SAUGUS ROUTE 107 OVER THE SAUGUS RIVER (BRIDGE NO. L-18-016 = S-05-016)

Design Consultant: STV, Inc.

Contractor: SPS New England, Inc.

Prepared by Chappell Engineering Associates, LLC
December 1, 2025

Ten Park Plaza, Suite 4160, Boston, MA 02116

Tel: 857-368-4636, TTY: 857-368-0655

www.mass.gov/massdot

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

Owner's Representative Contract Number: 120632

MassDOT Project Number: 604952

Construction Contract Number: 109691

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I. EXECUTIVE SUMMARY

The Belden G. Bly Bridge is a bascule bridge carrying Route 107 over the Saugus River between the Town of Saugus and the City of Lynn. It was originally constructed in 1912 to replace a timber bridge at the same location. A temporary bridge incorporating a moveable span was put into service in November 2013 immediately adjacent and to the east of the existing bridge, and the original bascule span removed, all under earlier, separate design and construction contracts. A new, heel-trunnion single span drawbridge is now nearing completion in the original roadway alignment.

A construction contract in the amount of \$87,523,965 was awarded to SPS New England in November 2020, followed by a Notice to Proceed on December 1, 2020. In year one (2021) the existing four superstructure spans and piers were removed, cofferdams installed at both abutments, the old abutments removed and the south abutment foundation piles installed. In year 2 (2022), the abutments at both approaches were constructed and, on the Saugus side, the Rest Pier trestle, the five drilled shaft foundations and the Rest Pier pier cap were installed. Work also advanced on the southwest retaining walls. On the Lynn side the trestle for access to the Bascule Pier workzone was installed. In year three (2023), the Bascule Pier drilled shafts and pier were constructed. The four tower columns and transverse trusses tying the two towers together were installed as were the two segmental gears and heel trunnion bearings, all on the Bascule Pier. Work on the control house and machine room advanced. The southwest retaining walls at the Saugus approach embankment completed. In year four (2024), the channel was closed on February 1 followed by installation of the two bascule girders, the floor beams and stringers, the grid deck, the lightweight deck concrete and the LMC overlay. The two counterweight girders were installed followed by the counterweight boxes and placement of counterweight concrete. A major project milestone was achieved on 5/13/24 when the bascule span was lifted into the vertical position for the first time and the channel reopened to marine traffic. Construction of Span 1, from the Saugus abutment to the Rest Pier, advanced in parallel with the bascule span construction, with installation of precast box beams, SIP forms, reinforcing steel and the deck pour. Installation of the operating machinery and of the core electrical systems for the moveable span also advanced. Reconstruction of Ballard St. began as well. In year five (2025), Span 3, from the Bascule Pier to the Lynn abutment, was constructed and the Lynn-side wingwalls and retaining walls were built. Construction of the control house is now essentially complete. Installation of the permanent power supply, delayed until an NGRID transformer was delivered, also completed. A second major project milestone was achieved on 11/20/25 when two-way traffic was taken off the temporary bridge and placed on the west side of the new bridge. The traffic shift created a workzone for the last remaining section of the southeast retaining wall; this work is expected to be ongoing through the winter and to complete next year in early spring. The demolition subcontractor is currently mobilizing for removal of the

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

Owner's Representative Contract Number: 120632

MassDOT Project Number: 604952

Construction Contract Number: 109691

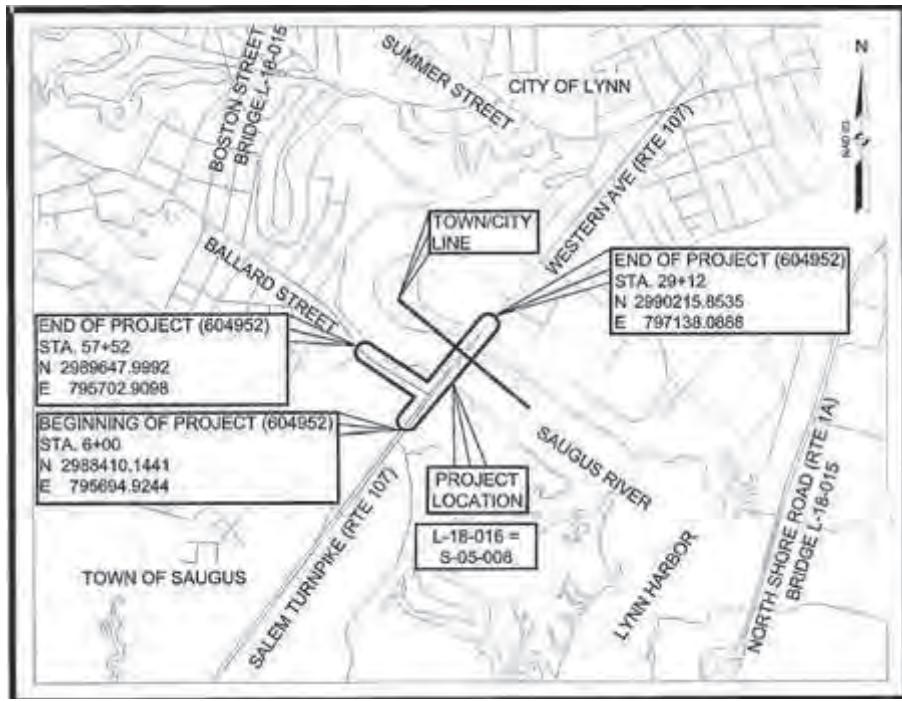
temporary bridge super and substructures; their work is expected to complete in March 2026. The civil work on Ballard St. and on Route 107 (drainage, full depth roadway construction, curb and sidewalk) advanced throughout the year and is also expected to complete next spring.

The most recent schedule update, progress as of 12/1/25, shows Contractor Field Completion milestone occurring on 7/16/26, 123 calendar days after the contractual completion date of 3/16/26. On the project to date, eighteen Extra Work Orders (EWOs) have been approved totaling \$3,017,846, of which seven were approved in 2025 totaling \$383,361. The most significant extra work order to date, EWO #3, was approved in June 2022 in the amount of \$1,913,312 for the removal of approximately 4,000 CY of stockpiled spoils material found to be co-mingled with asbestos. The most significant Materials Price Adjustment has occurred to item 999.4490, “Structural Steel – Positive”, totaling \$4,261,606 to date. At present, between approved and pending EWOs, materials price adjustments, overruns/underruns, and contract quantities remaining to be invoiced, ninety-nine percent (99%) of the Project Encumbrance (\$98,589,749 of the \$99,282,339) has been committed and \$692,590 is projected to remain at project completion (8% of the original contingency amount). Ninety-three percent (93%) of the current contract total (\$91,667,992 of \$98,534,552) has been invoiced while, as of December 1, eighty-nine percent (89%) of the contractual construction duration (1826 of 2045 calendar days) has elapsed.

II. ANNUAL REPORT

Contract Scope of Work

This project has replaced the temporary drawbridge, in service since 2013, with a heel-trunnion single-leaf bascule span. All structural, mechanical, electrical, power and communications systems components have been replaced in the alignment of the original bridge. The navigable channel was increased from 40' to 50' in width. The four fixed approach spans of the original bridge were replaced by one span to each side of the moveable span, and both approach roadways and the bridge have been reconstructed to accommodate two 11' travel lanes in each direction, two 6' bicycle lanes, and two 6.5' sidewalks. On Route 107 the southern project limit is about 1000' to the south of the reconstructed signalized Ballard Street intersection, and the northern limit approximately 700' to the north of the Lynn abutment, for an overall length along Route 107 of approximately 2400'. In addition, about 700' of Ballard Street is in the process of reconstruction.



Project Location: Route 107 over the Saugus River

Major Progress as of December 1, 2025

Year 5 of construction, (2025), has been an active year. Span 3, from the Bascule Pier to the Lynn abutment, was constructed over the winter months, with placement of the precast box beams in mid-January, formwork and reinforcing steel underway into March, and the deck pour taking place on 3/12/25.

Installation of the MWRA's submerged water main across the navigable channel also took place early in the year. The channel was closed on 1/20/25 with a commitment to the Coast Guard to re-open no later than 2/15/25. Major silt producing activities, among which channel excavation is included, are prohibited from 2/15 through 9/30. The trench was excavated, the water main floated into position, submerged, ballast blocks installed, and the trench backfilled; the channel re-opened on 2/14/25. The first section of the southeast retaining wall supporting the widened Saugus approach embankment was constructed in February and March. Then staged full depth roadway reconstruction of Rt. 107 and of Ballard St. began in March and is ongoing. Also, in late March the Contractor informed the Department of a critical path delay to the start of the northwest retaining wall foundation piles in Lynn, due to the deteriorated condition of the industrial building immediately to the west of the wall at 1147 Western Ave. The Department responded that the building was vacant and work should proceed. Pile driving, planned to last three weeks and to take place over the 2024/2025 winter, did not actually begin until late April 2025. Progress was intermittent due to obstructions and unknown utilities, and the work did not complete until eleven weeks later in mid-July. Pile driving then shifted to the northeast retaining wall, and both walls advanced concurrently as planned until substantially complete in early September. The Contractor's schedule updates from December 2023 through February 2025 had consistently showed the on-time completion of all milestones, however beginning with the issues at the northwest retaining wall, the updates have shown milestone delays which have varied for four to five months. Unfortunately, at this point in the project opportunities for schedule recovery before the contractual 3/16/26 Substantial Completion are limited. The Rt. 107 work, including full-depth roadway reconstruction, new drainage, curb and sidewalk, advanced slowly through the summer months, however by late August work on the center and southbound lanes was essentially complete. The Ballard St. work has also advanced slowly. This work, similar in scope to Rt. 107, has been divided into three stages, for the southern, center, and northern lanes. At present the southern and center roadway has been reconstructed and the southern curb and sidewalk installed. The northern lane has yet to be fully rebuilt, in part due to the installation of a new 12" water main by the town of Saugus within the northside workzone. Installation completed in November, however the workzone remains closed to traffic. In June work on the MWRA water main resumed, first in Saugus where the line was installed the length of the approach embankment, followed in August by the same work in Lynn. In September the 16" main was tied into existing MWRA piping at both ends. In November Spans 1 and 3 were paved as well as the approach roadways, the southbound warning and barrier gates were installed, and the westside curb and sidewalk at both bridge approaches constructed. On November 20 the project achieved a major milestone when traffic was removed from the temporary bridge and placed onto the two westside lanes of the new bridge. With traffic on the new bridge, full-depth reconstruction of the Route 107 northbound roadway began in both Saugus and in Lynn. Also, with traffic off the temporary bridge, the last remaining section of retaining wall

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

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on the east side of the Saugus approach embankment is expected to begin and to progress through the winter. At the end of the November the demolition subcontractor was mobilizing for removal of the temporary bridge superstructure and substructure. The demolition operation is a source of some concern as the last demolition activity, pier removal to 4' below the mudline, is currently shown to complete on 3/3/26 - however dredging to remove foundations is considered a major silt producing activity and is prohibited after February 15.

Budget and Current Financials

The low bidder, at \$87,523,965, was SPS New England, Inc. Notice to Proceed was issued on December 1, 2020.

Budget:

• Office Estimate:		\$84,037,952
• Bid Price:		\$87,523,965
• Original Encumbrance:		\$99,282,339

Original Encumbrance Obligation by Appropriation:

• 61210816 Federal Participating:	NHP(BR-ON)-003S (263)	\$98,373,657
• 61211317 Non-Federal Aid:	100% State	<u>\$908,682</u>
		\$99,282,339

Breakdown of Original Encumbrance and Allowance Items:

• SPS New England, Inc. Bid:		\$87,523,965
• Contingencies:		\$8,752,774
• Traffic Police:		\$2,100,000
• Traffic Police OT/Non-Participating:		\$900,000
• Trainees:		<u>\$5,600</u>
		\$99,282,339

Current Project Financials (as of December 1, 2025):

	Contract	MassDOT
i Bid	\$ 87,523,965	\$ 87,523,965
i Allowances	\$ 3,005,600	\$ 3,005,600
i Original Contract Value	\$ 90,529,565	\$ 90,529,565
i Original Contingency (FIN681)		\$ 8,752,774
i Original Encumbrance		\$ 99,282,339
i Encumbrance Modifications		\$ 0
i Current Encumbrance		\$ 99,282,339
i Current Overruns and Underruns	\$ 2,053,855	\$ 2,053,855
i Approved Contract Modifications	\$ 5,951,132	\$ 5,951,132
i Current Contract Value	\$ 98,534,552	\$ 98,534,552
i Pending Contract Modifications	\$ 55,197	\$ 55,197
i Estimated Contract Total	\$ 98,589,749	\$ 98,589,749
i Probable Contract Modifications		\$ 0
i Forecast Cost-at-Completion	\$ 98,589,749	\$ 98,589,749
i Total CQE (Invoiced) to Date	\$ 91,677,992	\$ 91,677,992
i Funds Remaining		\$ 7,604,347
i Projected Funds Remaining		\$ 692,590
i Expected Progress (baseline)	97.93%	

Schedule/ Project Milestones

As noted in the Major Progress as of December 1, 2025 section above, over the fifteen-month period from December 2023 through February 2025, the Contractor's schedule updates indicated on-time completions for both MS-01, Completion of Stage 4 Operational Support, and MS-02, Contractor Field Completion. However, as a result of issues related to the northwest retaining wall, since March 2025 the updates have consistently shown milestone delays of from four to five months. The District review of the March schedule update indicated that the Contractor should either submit a Preliminary Time Entitlement Analysis (PTEA) to demonstrate excusable delay, or recover the projected delay. In early May the Contractor requested that any overtime worked to recover the delay be reimbursed as extra work. On 5/15/25 the Department denied the request and again requested submittal of a Preliminary Time Extension Analysis to demonstrate excusable delay. On 6/18/25 the Contractor submitted the analysis and the District review, posted 7/25/25, deemed the submittal "For Information Only" as it did not conform to the contract requirements with respect to timeliness, content, and methodology. The Contractor was directed to "monitor progress" over the next several months after which if it believes it is entitled to an excusable extension of time, to submit a Final

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Time Extension Analysis (FTEA), having first addressed the comments on the PTEA. It is unclear at this time if a resubmittal of the PTEA or submittal of a FTEA will be forthcoming. The most recent schedule update (Update #59, as of 12/1/25) shows MS-02 Contractor Field Completion on 7/17/26 (contractual completion: 3/16/26), and MS-01 Completion of Stage 4 Operational Support on 7/16/27 (contractual completion: 3/16/27). Unfortunately, this late in the project, opportunities for schedule recovery are very limited.

Areas and Issues of Concern

The remaining major construction activities are limited to demolition of the temporary bridge and construction of the final section of the southeast retaining wall. Neither is complex and, in our opinion, do not present exceptional risks.

In September 2025, the Contractor submitted a total of seven “Notice of Claim/Step 1 Meeting Request” letters for issues where EWO requests had previously been rejected by the Department. Combined, the requests total \$279,832, or 40% of the remaining uncommitted contingency funds. It is likely that the requests will be grouped and addressed jointly early in 2026. There are fundamental shortcomings with respect to notice and timeliness requirements for several of the requests.

While it is unlikely that the project will complete within the contractual execution period, barring unforeseen circumstances we expect that the project will complete within the original Project Encumbrance.

Progress Photographs



Placement of concrete ballast blocks on 20" MWRA
water main
January 2025



Span 3 reinforcing and SIP pans
January 2025

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

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Span 3 reinforcing steel detail
January 2025



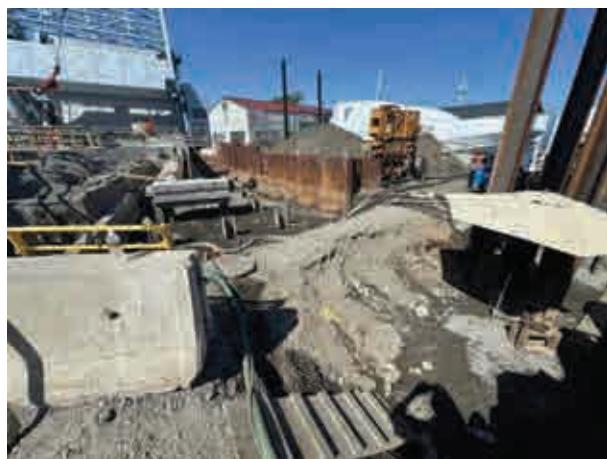
Machine house interior
January 2025



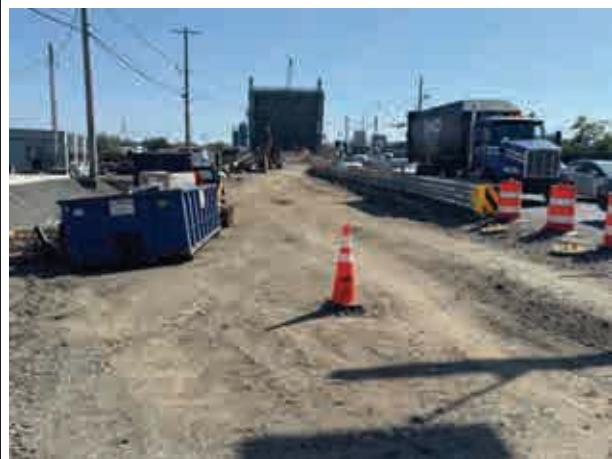
Looking north: control house with open bascule span
April 2025



Span 3 east side sidewalk cure from Lynn abutment
April 2025



Northwest retaining wall partially complete
June 2025



Looking north: Rt. 107 SB full depth reconstruction
June 2025

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

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Looking south: Rt. 107 SB full depth reconstruction
June 2025



Ballard St. Stages 1A and 1C complete
August 2025



Looking east: Saugus approach, Span 1, raised bascule
August 2025



Looking south: Span 3, control house, raised bascule
August 2025



Looking north with traffic switched onto the new bridge
December 2025



Looking north: full depth roadway reconstruction Rt.
107 northbound in Lynn
December 2025

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

Owner's Representative Contract Number: 120632

MassDOT Project Number: 604952

Construction Contract Number: 109691



III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

The peer review requirements for this project were satisfied during the design phase and are continuing during the construction phase. Presently, the undersigned OR is satisfying the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed and walking the site to observe major construction activities.

Value Engineering

The VE Workshop was conducted by Jacobs Engineering Group and held over a one-week period in November 2016. A Preliminary VE Report was submitted to the Department in early December 2016 with thirty-four alternatives identified. Of the thirty-four proposed, eight were accepted, seven conditionally accepted and nineteen rejected. The final VE Study Report was forwarded to FHWA on 11/10/17 for record. There have been no VE initiatives to date by the Contractor.

Cost Recovery

On 10/20/22 EWO#5 in the amount of \$21,375.42 was approved "for shop modifications to the counterweight trunnion bore hole that were required in order to correct an error in the counterweight girder camber table, which provided incorrect deflections". The girder was fabricated based on the approved shop drawings and design documents however the erroneous deflection values given in the plans were significant

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enough to impact the center of gravity of the counterweight assembly and it was determined that adjustment of the two trunnion hub bore locations was the most cost-effective way to address the issue. A Cost Recovery Inquiry Form was initiated in February 2023, and the Review Panel convened and agreed to advance the issue to the Cost Recovery Standing Committee. The consensus of the Standing Committee, in their 4/5/23 meeting, was that design error on the part of the Engineer of Record (EOR) was solely responsible for the extra work. A letter was sent on 7/10/23 to the EOR requesting either reimbursement or in-kind services at no cost to MassDOT, or that a meeting be requested should the EOR wish to present additional information. To our knowledge, to date there has been no response.

Interferences were encountered during the initial 5/13/24 bridge opening between the sidewalk heel joint cover plate on the moveable span and the bascule pier sidewalk at both sidewalks. As a result the span could only be elevated to 65 degrees, 13 less than the design 78 degrees. The issues were documented in RFI#221, "Sidewalk Heel Joint Interference with Heel Bearing, Tower and Sidewalk", submitted in late May. The RFI response, initially posted in June and updated in November, acknowledged the interferences and proposed demolishing a section of the movable span concrete at the sidewalks in order to move the armored joint several inches into the moveable span. A Notice of Change dated 8/13/24 was received from SPS followed by an EWO request in the amount of \$71,902. A Cost Recovery Inquiry Form was initiated in February 2023, the Review Panel forwarded the issue to the Standing Committee, and, in their 5/4/25 meeting, the Committee found EOR design error to be responsible for the full cost of the extra work. The letter requesting either reimbursement or in-kind services at no cost to MassDOT, or that a meeting be requested, was sent on 10/15/25; to our knowledge to date the EOR has yet to response.

No other issues have been identified to date as potentially cost recoverable.

An initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due, as a formal evaluation process must still be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for referral to the Cost Recovery Standing Committee. In order to perform the legislative mandate of being the Primary Manager of Cost Recovery, the OR will follow MassDOT Highway Division's Cost Recovery Procedure (SOP No. CSD 25-14-1-000, issued 11/1/22).

Oath

I, Richard B. Littlefield P.E., MA P.E. No. 38773, hereby certify that my sole responsibility as Owner's Representative, under OR Contract No. 120632, is to the Commonwealth of Massachusetts and MassDOT.

Project Name: Lynn-Saugus Bridge Replacement – Route 107 over Saugus River

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Construction Contract Number: 109691

Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.



Richard B. Littlefield, P.E.

Date: December 1, 2025



Mauro Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project No. 606552



Owner's Representative's 2025 Annual Report

OR Contract No.: 120631 Fuss & O'Neill

*Project No.: 606552: Northampton – Bridge Replacement N-19-059
Interstate I-91 Over US 5 and B&M Railroad; and N-19-060 Interstate Over
Hockanum Road; and Improvements to Interstate 91 Interchange 18*

Contractor: J.F. White

Designer: Parsons

Prepared by: Fuss & O'Neill/Bayside Engineering, Inc.

December 16, 2025

Ten Park Plaza, Suite 4160, Boston, MA 02116

Tel: 857-368-4636, TTY: 857-368-0655

mass.gov/massdot

www.mass.gov/massdot

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552

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**NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18**
Project No. 606552

EXECUTIVE SUMMARY

The two bridges (4 structures in all) carrying Interstate 91 both Northbound and Southbound were constructed in 1965 and have not been reconstructed. (Please refer to Page 4 for Project Orientation) The Northbound and Southbound bridges over Route 5, etc. are each about 416 feet in length with 6 spans and are made up of cast-in-place concrete decks over steel stringers. The bridges over Hockanum Road are each about 83 feet in length and are made up of cast-in-place concrete decks over steel stringers in one continuous span.

Based on inspections completed in early 2020, the conditions of the substructures were all determined to be level 4 which equates to poor condition with advanced deterioration and spalling. The condition of the superstructure of I-91 SB over Route 5, etc. was assessed a 3 which equates to Serious Condition – Loss of section, deterioration, spalling, or scour have seriously affected primary structural component. The superstructure for the I-91 NB over Route 5, etc. was assessed as a 4 (see above). The superstructure for I-91 SB over Hockanum Road was assessed a 5 which equates to Fair Condition and the I-91 NB over Hockanum Road was assessed a 6 which means Satisfactory Condition.

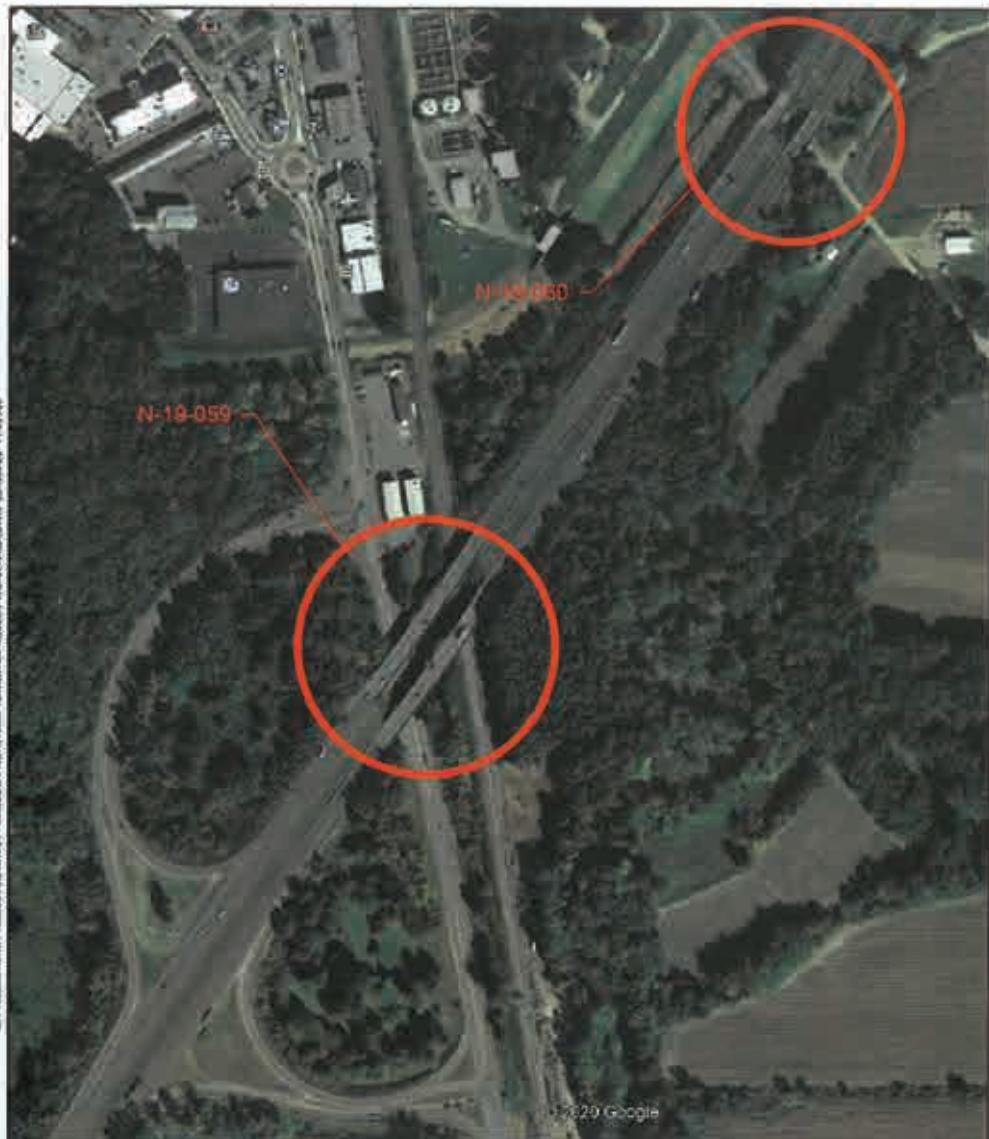
The two I-91 SB bridges carry an average daily traffic count of more than 29,000 vehicles per day and the two I-91 NB bridges carry roughly 24,000 vehicles per day.

At the onset of the project, US Route 5 did not meet the current Healthy Transportation Initiatives goals. This project will construct a sidewalk on the west side of the roadway and provide bicycle accommodations on both sides of the roadway, as well as provide crosswalks at the ramp intersections. Widening of turning lanes will be provided and also traffic signals, new signage, and pavement markings will be installed.

During construction, I-91 traffic will be maintained through the use of a temporary roadway and Acro Panel bridge through the median. Also, temporary widened ramps will be utilized for on-ramp and off-ramp motorists while construction of the permanent ramps is completed.

The project was designed by Parson's and bids were opened on December 8, 2020. The project had an estimated construction duration of nearly 30 months. The winning bidder was J.F. White with a low bid of \$42,115,135.60.

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552



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PROJECT WORK AREA

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552

ANNUAL REPORT

Contract Scope of Work:

The project consists of the complete reconstruction of the superstructures and substructures of four bridge structures carrying Interstate 91 Northbound and Southbound over US Route 5 and the B&M Railroad; and Interstate 91 over Hockanum Road. Traffic will be conveyed through this section by constructing a temporary two-lane roadway and building two temporary bridges for a distance of nearly 4400 linear feet in the median area. Temporary on-ramps and off-ramps will be constructed as well.

Also as part of the project, US Route 5 will be reconstructed including a new drainage system, addition of a sidewalk on the west side of the road, bicycle lanes on both sides of the roadway, and new traffic signals. The project includes safety improvements to I-91, US Route 5, and the ramp systems. Route 91, US Route 5, and Hockanum Road will all be milled and overlayed with hot mix asphalt.

Major Progress As Of November 30, 2025

For the year, with everything being opened to traffic by the end of last year, minor items were completed including the following:

- An Extra Work Order to custom design and install specialty guard rail to protect motorists from the Pier 2 columns and downspout on Route 5 Northbound was completed. This involved designing specialty posts and a concrete base due to interference with underground utilities in the same area.
- An Extra Work Order to design and re-install downspout drainage system which originally dumped out onto the sidewalk on Route 5 Southbound.
- Replacing failing bearings.
- Minor housekeeping items including caulking barrier, catch basin cleaning, and rubber joint sealing.

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552

Budget:

Bid Price: \$42,115,135.60

Estimated Contract Cost: \$44,920,736

Encumbered* Amount: \$49,133,724.00

*Includes Contingencies, Railroad Flaggers, Traffic Police, Specialty Services, and Test Pit Excavations.

Encumbrance Modification Amount: \$1,033,967

Encumbrance After Modification: \$50,157,691

Total Amount Expended through Nov. 1, 2025: \$49,219,550

Percent Completion to Date: 98.98

Current Estimate at Completion: \$49,725,049

Schedule/Project Milestones:

From NTP On 2/8/21 to Contractor Field Completion scheduled for 12/5/24, the Estimated Construction Duration was 1419 days. That has been extended to 12/28/2024 making it 1442 days.

This Contract contains the following Contractual Milestones:

Milestone #6 - Traffic on Temp I-91 NB

454 Days from NTP (5/7/22) Date extended to 5/20/22 which contractor met.

Milestone #5 - Full Beneficial Use I-91 NB (Completed Bridge/ Traffic on New I-91 NB)

854 Days from NTP (6/11/23) – Date extended to 6/18/23 which contractor met.

Milestone #4 - Full Beneficial Use I-91 SB (Completed Bridge/ Traffic on New I-91 SB)

1,261 Days from NTP (7/22/24) has been extended to 8/05/24– Contractor met this milestone on 6/20/24.

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552

Milestone #3 - Full Beneficial Use Incl. RT 5 & Hockanum Rd

1,332 Days from NTP – (10/1/24) – Contractor did forecast this at 9/09/24, and then 11/5/24, and then for 6/9/25. Full Beneficial Use is defined as: The majority of contract Work has been completed and the asset(s) has been opened for full multi-modal transportation use, except for limited contract work items that do not materially impair or hinder the intended public use of the transportation facility. All anticipated lane takings have been completed, except for minor, short term work items.

During the placement of guardrail behind the sidewalk on Route 5, it was discovered that there is a conflict with the embedment length of the posts being limited by the footing from Pier 2. Also, stemming from a final Bridge Section inspection, the downspout from scuppers at Pier 1 emptied onto the sidewalk. These two items that required design, installation, and pricing were finally all complete in September 2025.

Milestone #2 - Substantial Completion

1,353 Days from NTP (10/22/24) has been extended to 11/12/24 – Contractor had most recently 8/13/25 due to the conflicts described above. Substantial Completion is defined as: A walkthrough of the entire contract Work has been performed by the Resident Engineer, a Punch-list has been generated and the Work required by the contract, including paper work, has been completed, except for work having a contract price of less than one percent of the adjusted total contract price, including overruns, underruns and all contract amendments. All material submittals have been received by the District Materials Lab.

Milestone #1 - Contractor Field Completion

1419 Days from NTP (12/28/24) – Contractor most recently forecast this for 9/25/25 due to the conflicts described above. Contractor Field Completion is defined as: All physical contract work is complete including punch-list. The Contractor has fully demobilized from field operations.

Areas and Issues of Concern

The only issue remaining is regarding final payments for work completed and submission of related paperwork.

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and Improvements to Interstate 91 Interchange 18
Project No. 606552

Project Photographs



Specialty Guard Rail Foundations and Posts at Pier 2 (Route 5 NB)

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552



Prior Condition-Downspout at Pier 1 Emptying Onto Sidewalk

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
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Re-worked Downspout at Pier 1 Emptying Behind Sidewalk and Guard Rail

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552

CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

As part of the initial design phase for this project, a peer review was performed at each stage of the design by MassDOT's Highway Division forces independent of the project designer. These reviews were performed to identify conflicts or errors that should be corrected or mitigated at each stage before design progressed further. The O.R. was not engaged with this project during the design and thus did not participate in the peer review of the design by reviewing and commenting on the consultant's peer review report, attending project review meetings, and design milestone reviews.

During the construction phase of this project, this O.R. satisfied the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

Value Engineering

It is the Owner's Representative's understanding that no Value Engineering process was utilized on this Project.

Cost Recovery

One situation that may rise to the level of cost recovery entails when it was noticed that there were somewhat significant discrepancies between the survey provided and the design plans. This issue came to light while attempting to install drainage structures on Route 5. The design for this area is to essentially meet existing grade but the structures when installed were sticking out of the pavement by as much as 2.5 feet. The contractor suspended operations on the Route 5 work from late September until early/mid November of 2021 when revised numbers were provided by the Design Consultant. They still continued to work on the temporary roadway in the median which is part of the Critical Path to completion for the project. The contractor was given additional days on the contract for this situation and a settlement was negotiated for \$50,132.53 (.1% of projected project cost).

There have been overruns of quantities primarily relating primarily to the calculations for earthwork items in the median temporary bypass roadway.

NORTHAMPTON– Bridge Replacement N-19-059 Interstate I-91 Over US 5 and
B&M Railroad; and N-19-060 Interstate Over Hockanum Road; and
Improvements to Interstate 91 Interchange 18
Project No. 606552

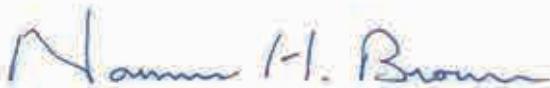
It does not appear that these rise to the level of errors leading to cost recovery with conventional industry thinking being that if these quantities were included in the original contract, the cost to the contract would have been that much more at bid time.

It should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for its referral to the Cost Recovery Standing Committee. For any issue referred for Cost Recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 8/7/17) shall govern. In order to perform the legislative mandate of being the Primary Manager of Cost Recovery, the Owner's Representative will follow the referenced SOP. This SOP, as modified for projects with an Owner's Representative, describes the role of the Owner's Representative and includes a Cost Recovery Standing Committee. This committee oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary.

Insurance notifications occur only once a final decision has been made.

OATH

I, Norman H. Brown, P.E., MA P.E. No. 36531, hereby certify that my sole responsibility as Owner's Representative, under OR Contract 80661, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

A handwritten signature in blue ink that reads "Norman H. Brown". The signature is fluid and cursive, with "Norman" in blue and "H. Brown" in red.

Norman H. Brown, P.E., P.L.S.

Date: December 16, 2025



Maure Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

***Owner's Representative Contract No. 120632 – Chappell Engineering Associates, LLC
Project No. 605888 – Route 24/Route 140 Interchange Improvements,
TAUNTON, MASSACHUSETTS***

***Designer: Stantec Consulting Services, Incorporated
Prime Contractor: Manafort Brothers Incorporated***

***Prepared by: Chappell Engineering Associates, LLC
December 16, 2025***

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I. EXECUTIVE SUMMARY

This project consists of improvements to the interchange of two state highways in southeastern Massachusetts - Route 24 and Route 140 (Exit 17), located in the City of Taunton. The project includes improvements to 6,500 linear feet of Route 24, 4,200 linear feet of Route 140 and improvements to the interchange ramp system that connects the two roadways. Improvements are also proposed at the nearby Route 140 at Industrial Road intersection to the west, and the Route 140 at Stevens Street interchange. A locus map of the study area is shown in Figure 1. Stantec Consulting Services, Inc. (Stantec) is the designer of record. The project was advertised for construction in August of 2020 and bids were opened November 24, 2020 with the low bidder being Cardi Construction Industries of Warwick Rhode Island. Notice to Proceed was issued to Cardi on February 17, 2021. Work on the site began in the Spring of 2021 and continued through the summer of 2023 when Cardi suspended work on the project which effectively ceded the project to the surety bonded to the project; Everest. On December 5, 2023 MassDOT's Legal Section reached an agreement with the surety and, an agreement was reached where Manafort Brothers Incorporated (MBI), the surety's replacement contractor will complete the project in accordance with the terms of the contract. MBI began work on the site in January 2024 and has since assumed construction activity throughout the project, making significant progress in all available work zones and is working towards a shift into the second stage of construction which involves moving traffic onto newly constructed bridge and roadway infrastructure on both Routes 24 and 140.

The design and construction of this roadway project has been closely coordinated with MassDOT's South Coast Rail Project. In February of 2025, the newly constructed Ramp F was opened to traffic traveling making a direct connection from Route 24 southbound to Route 140 northbound and the East Taunton Train Station which was made open for use by the public in March of 2025.

The purpose of this project is to address existing and future (2040) safety and operational deficiencies at the Route 24/Route 140 interchange and the nearby vicinity. Historically, users of the Route 24/Route 140 interchange have experienced significant vehicular delay, especially for the movements from Route 24 southbound to Route 140 southbound and from Route 140 northbound to Route 24 northbound. Future impacts from the South Coast Rail Project and other proposed developments, such as the Liberty & Union Industrial Park located to the east, are projected to cause conditions to deteriorate further. Proposed improvements within this project specifically address impacts from these, and other, planned developments, as well as standard regional growth.

With regards to safety, the existing structure carrying Route 24 over Route 140 is structurally deficient and does not satisfy vertical clearance requirements. At approximately 1,000 feet in

length, the existing acceleration lane for Route 24 southbound on-ramp traffic from Route 140 is also substandard. MassDOT has identified an existing Highway Safety Improvement Program (HSIP) crash cluster at the interchange, an indication that the interchange is in the top 5% of high accident locations in the Southeastern Regional Planning and Economic Development District (SRPEDD) region. In 2014, MassDOT initiated a Road Safety Audit (RSA) to identify existing safety-related issues and target both short and long term solutions to address these issues. The RSA Team was comprised of personnel from the Taunton Public Works Department, City of Taunton Police and Fire Departments, the Massachusetts State Police, MassDOT and Stantec. Most of the recommendations of the RSA are incorporated into the project design.

Improvements proposed as part of this project include:

- New Route 24 Southbound Ramp to Route 140 Northbound/Industrial Drive (Exit 17B);
- Widening of Route 24 to three travel lanes in each travel direction, plus acceleration and deceleration lanes at interchange ramps;
- Reconstruction of interchange ramps between Routes 24 and 140, including two lane entrance ramps from Route 140;
- Reconstruction of two bridges along Route 24 (over Route 140 and over the railroad);
- New bypass lane on Route 140 southbound, between the southbound ramps and Stevens Street;
- Widening of Route 140 to accommodate additional turning lanes and the bypass lane.

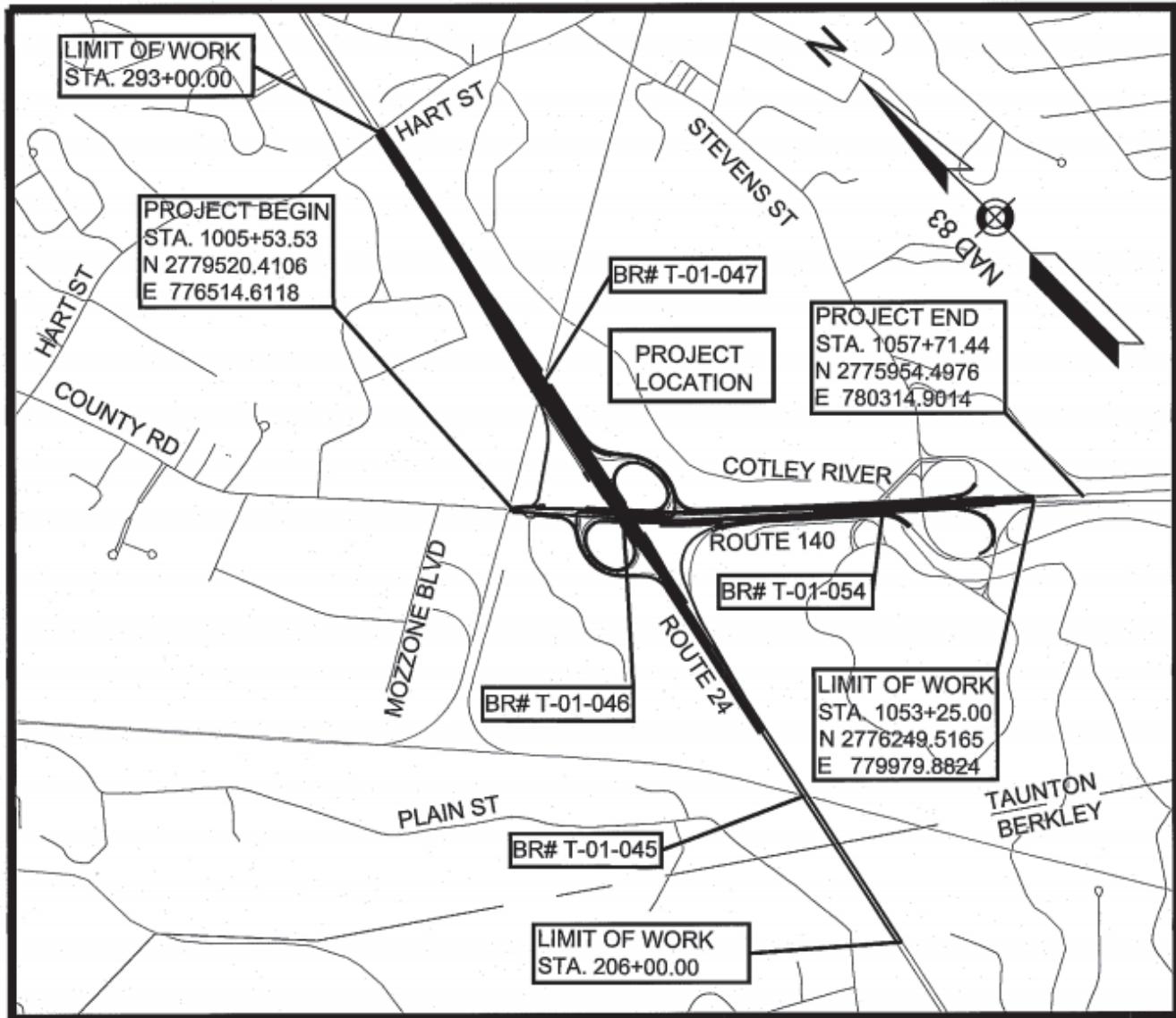


Figure 1 - Project Location

II. ANNUAL REPORT

Contract Scope of Work

Please refer to Figure 2 for a graphic depiction of the ramp and wall nomenclature. Proposed improvements include:

- Widening Route 24 from two to three travel lanes in each travel direction, plus acceleration and deceleration lanes at interchange ramps. Two bridges are replaced with the widening: Bridge T-01-047 (over the MBTA Middleborough Secondary line) and Bridge T-01-046 (Route 24 over Route 140). Bridge T-01-045 (over the MBTA South Coast Rail Stoughton trunk line) will also be replaced. Design of this structure and approach roadways was performed as part of the South Coast Rail Project and incorporated into this contract given its proximity and relative timing. The bridge work will include the reconstruction of the superstructure and substructure and will occur in stages to avoid disruption to traffic.

Widening Route 24 and replacing the bridges eliminates existing substandard design elements and capacity constraints of mainline highway segments.

- The Route 24 southbound exit ramp at Route 140 (Exit 17) will be split into Exit 17A and Exit 17B with the construction of a new off-ramp serving Route 140 northbound. This new off-ramp (Ramp F) will terminate at Route 140 across from Industrial Drive, the access point to the proposed East Taunton commuter rail station. The existing Route 24 southbound off-ramp serving Route 140 southbound will be reconstructed and extended into a bypass lane providing continuous and exclusive travel from Route 24 southbound all the way to the Stevens Street interchange (Route 140 Interchange Exit 19) where this bypass lane joins mainline Route 140 southbound.

Splitting the Route 24 off-ramp into two ramps allows the Route 140 northbound traffic to exit “ahead of” the much heavier Route 140 southbound traffic. Route 140 southbound traffic no longer terminates at the Route 140 intersection causing delays on Route 24, but bypasses it altogether until Stevens Street. The intersection of Route 140 at the Route 24 southbound entrance ramp will be reconstructed to serve only this movement. This proposed reconfiguration, in conjunction with the construction of Ramp F (described above) will allow for the elimination of a traffic signal phase at the ramp terminal at-grade intersection, as it would no longer be necessary to provide a traffic signal phase for exiting ramp traffic from Route 24 southbound.

- Reconstruction and reconfiguration of the following ramps, to provide increased capacity and meet future design requirements:
 - Route 24 Southbound Entrance Ramp from Route 140 Southbound (Ramp A)
 - Route 24 Northbound Exit Ramp to Route 140 Northbound (Ramp D)
 - Route 24 Northbound Entrance Ramp from Route 140 Northbound (Ramp E)
- Widen Route 140 to two through travel lanes in each direction, supplemented by auxiliary turn lanes, acceleration lanes, deceleration lanes and weaving lanes to or from Route 24 or Stevens Street. Route 140 will also be widened to accommodate an expanded cross section that features up to seven lanes between the Route 24 northbound and southbound ramps. The expanded cross section includes two auxiliary left turn lanes, two northbound lanes, two southbound lanes and a southbound bypass lane (from Route 24 southbound). The southbound bypass lane is separated from mainline Route 140 by a cement concrete median barrier. The widening of Route 140 will include the extension of the existing Cotley River Culvert (T-01-054) to accommodate the widening.
- The improvements on Route 140 include the full reconstruction of the traffic signals at the following three intersections: Route 140 at Route 24 southbound off-ramp/Industrial Drive (designed by Vanasse Hangen Brustlin (VHB) as part of the South Coast Rail Project), Route 140 at Route 24 southbound on-ramp, and Route 140 at Route 24 northbound on-ramp.
- Widening the existing Route 140 southbound off-ramp to Stevens Street (Ramp H) to allow for traffic using the ramp to access County Street and Stevens Street. This reconfiguration is expected to reduce weaving movements on Route 140 southbound, as it allows through traffic on Route 140 southbound to access County Street via the Exit 19A ramp. Under existing conditions, traffic towards those destinations would have had to use the Exit 19B ramp. The Route 140 southbound off-ramp approach will consist of two 12-foot wide travel lanes (one dedicated auxiliary right-turn and one shared left/through lane), with 2-foot wide left and right shoulders. The dedicated auxiliary right turn lane is proposed to be approximately 400 feet in length. The reconfigured off-ramp is proposed to align directly across Galleria Mall Drive. As a result of the reconfiguration of the ramp, the traffic signal system at the intersection is proposed to be reconstructed.



Figure 2 – Wall and Ramp Key Plan

Major Progress As Of November 30, 2025

Bids were opened on November 24, 2020 with the low bidder being Cardi Corporation of Warwick, Rhode Island. Notice to Proceed was issued to Cardi Corp on February 17, 2021. Bi Weekly project meetings have been held consistently on Tuesday mornings beginning with the first held on April 13, 2021. Work on the site began in the Spring of 2021 and continued through the summer of 2023 when Cardi suspended work on the project. On December 5, 2023 MassDOT's Legal Section reached an agreement with the surety and, an agreement was reached where Manafort Brothers Incorporated (MBI), the surety's replacement contractor will complete the project in accordance with the terms of the contract. MBI began work on the site in January 2024 and has since assumed construction activity throughout the project, making significant progress in all available work zones and is working towards a shift into the second stage of construction which involves moving traffic onto newly constructed bridge and roadway infrastructure on both Routes 24 and 140.

The following construction activities have been accomplished in 2025:

Route 24 Corridor

- Installation of drainage, signage and highway lighting in the stage 1B widening workzone.
- Construction of Stage 1B roadway widening and retaining walls to make provision for two lanes of travel as the project progresses into Stage 2 of construction.
- Construction of Stormwater Control Measures adjacent to roadway widening east of Route 24.

Route 140 Corridor

- Installation of drainage, signage and highway lighting in the stage 1B widening workzone
- Installation of underground utility corridor infrastructure between Ramps C and G.
- Completion of widening of Route 140 southbound between Ramps C and G.
- Completion of widening of Route 140 southbound between Ramps G and H.
- Completion of widening of Route 140 northbound between Ramps I and E.

- Continued construction of roadway widening. Installation of traffic signals at the Route 140 at Industrial Drive and Ramp F intersection.
- Continued construction of footings for overhead signs.

Route 24 bridge over the South Coast Rail (T-01-045)

- Construction of Stage 1B abutments and superstructure. This includes the provision for two lanes of travel on the newly widened portion of the bridge.
- Construction of roadway approaches to the structure and retaining walls 10 and 2.

Route 24 Bridge over Route 140 (T-01-046)

- Construction of Stage 1B abutments and superstructure. This includes the provision for two lanes of travel on the newly widened portion of the bridge.
- Construction of roadway approaches to the structure and retaining wall 3.
- Construction of temporary transition from Route 24 to Ramp D connecting to Route 140 northbound.

Route 24 bridge over MBTA/CSX (T-01-047)

- Construction of Stage 1B abutments and superstructure. This includes the provision for two lanes of travel on the newly widened portion of the bridge.
- Construction of roadway approaches to the structure and retaining wall 11.

Route 140 over the Cotley River (T-01-054)

- Cotley River extension completed south of Route 140.
- Cotley River extension completed north of Route 140.
- Removal of temporary water controls and Cotley River flow restored to normal alignment.

Ramp Construction

- Completion of ramp construction and opening of the ramp for public travel from Route 24 southbound to Route 140 northbound and the East Taunton Train Station.

- Completion of Stage 1B connection from Route 24 northbound to Route 140 southbound.
- Completion of Stage 1B connection of Ramp E from Route 140 northbound to Route 24 northbound.
- Completion of Stage 1B connection of Ramp D from Route 24 northbound to Route 140 northbound.

Budget

- Office Estimate: \$135,152,924.00
- Bid Price: \$116,498,484
- Encumbered Amount: \$134,471,198
- Current Encumbered Amount: \$134,471,198
- Total Amount Expended through November 30, 2025: \$ 50,363,032
- Current Estimate at Completion: \$ 124,426,026

The table below contains a summary of project financials through November 30, 2025:

	Contract	MassDOT	Municipal
① Bid	\$ 116,498,484	\$ 116,498,484	\$ 0
② Allowances	\$ 5,220,033	\$ 5,220,033	\$ 0
③ Original Contract Value	\$ 121,726,517	\$ 121,726,517	\$ 0
④ Original Contingency (RM681)		\$ 12,744,681	
⑤ Original Encumbrance		\$ 134,471,198	
⑥ Encumbrance Modifications		\$ 0	
⑦ Current Encumbrance		\$ 134,471,198	
⑧ Current Overruns and Underruns	\$ -494,665	\$ -494,665	\$ 0
⑨ Approved Contract Modifications	\$ 2,100,267	\$ 2,100,267	\$ 0
⑩ Current Contract Value	\$ 124,321,348	\$ 124,321,348	\$ 0
⑪ Pending Contract Modifications	\$ 10,476	\$ 10,476	\$ 0
⑫ Estimated Contract Total	\$ 124,331,823	\$ 124,331,823	\$ 0
⑬ Probable Contract Modifications		\$ 94,209	
⑭ Forecast Cost-at-Completion	\$ 124,426,026	\$ 124,426,026	\$ 0
⑮ Total COE (Invoiced) to Date	\$ 50,363,032	\$ 50,363,032	\$ 0
⑯ Funds Remaining		\$ 84,160,165	
⑰ Projected Funds Remaining		\$ 10,045,171	
⑲ Expected Progress (baseline)	0.00%		
⑳ % Complete	40.58%		

Schedule / Project Milestones

- Notice to Proceed: February 17, 2021
- Ramp F Full Beneficial Use: February 6, 2025 (472 Days Late)
- Full Beneficial Use: May 1, 2028 (431 Days Late)
- Interim Substantial Completion: July 7, 2028 (435 Days Late)
- Substantial Completion: October 6, 2028 (337 Days Late)
- Contractor Field Completion: November 1, 2028 (342 Days Late)

The surety and MBI have produced a new baseline schedule which reflects current project schedule and impacts to contract milestones.

A Time Extension Analysis was submitted on December 5, 2022 and reviewed by the MassDOT District Schedule Engineer with comments returned January 30, 2023. The District review of the schedule concurred with a portion of the request and determined that the Contractor should be eligible for an extension of 170 days of time for the completion milestones established in the contract documents. The updated accepted schedule milestones reflect the extended time as agreed upon.

The schedule delays which are considered valid by the District Reviewer have been attributed to the following:

- Delays in procuring structural steel as required to complete the median portion of bridge T-01-046 and thus delaying Stage 1A.
- Unforeseen lack of pavement structure under the Route 24 paved shoulder resulting in the need to relocate barrier, sawcut and increase the median construction that the Stage 1B roadway pavement would be adequate for public use during this stage.

The time between the abandonment of the project by Cardi Corp in July of 2023 and the formal execution takeover agreement (approximately 180 days) has been generally accepted as a non recoverable period of time for the project. When combining the previously accepted delay of 170 days with the takeover delay of 180 days, the current schedule forecast is tracking consistently.

The Contractor has been asked to produce a new baseline schedule where potential recovery can be evaluated. This is currently in progress as the TEA is prepared to correlate the updated baseline schedule with approved delays.

Areas of Concern

The following represents the primary areas of concern identified in design and at this stage in construction:

- The project schedule had fallen behind the contractual milestones prior to abandonment and has been put on hold for the time since the site was abandoned. The milestones of substantial completion and contractor field completion are the same as they were prior to the site abandonment by Cardi. This will need to be monitored closely and it will be weather dependent to maintain the schedule. It is crucial that the project be able to move into Stage 2 prior to the weather shutting down roadway work. Once in Stage 2, the contractor can continue to progress the project work with demolition of the existing structures through the winter season.
- Utilities: Utility relocations continue to be performed in conjunction with project staging. This will need to be carefully performed within each stage.
- Railroad access and foul times: staging bridge construction over the Railroad Bridges continues to be complex due to the limited hours of operation and multiple stages of construction. This has been further complicated by the transition of flagging service from MassCoastal to Keolis. The South Coast Rail is in full service and flagging has been consistent to this point allowing construction to proceed at the two railroad bridges. It is imperative that this continue in order to maintain the project schedule.
- Maintenance of Traffic/Construction Staging: construction staging and maintenance of traffic continues to be a very challenging element of the project due to heavy traffic volumes with four separate stages of construction and bridge replacements. The project is currently transitioning from Stage 1B to 2. Stage 2 work is in the middle portion of the Route 24 and Route 140 Roadways, making access challenging for construction.
- Construction fatigue for the motoring public traveling through the work zone which is projected to extend into 2028.

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402

Project Figures / Photographs



Bridge T-01-047 North Abutment

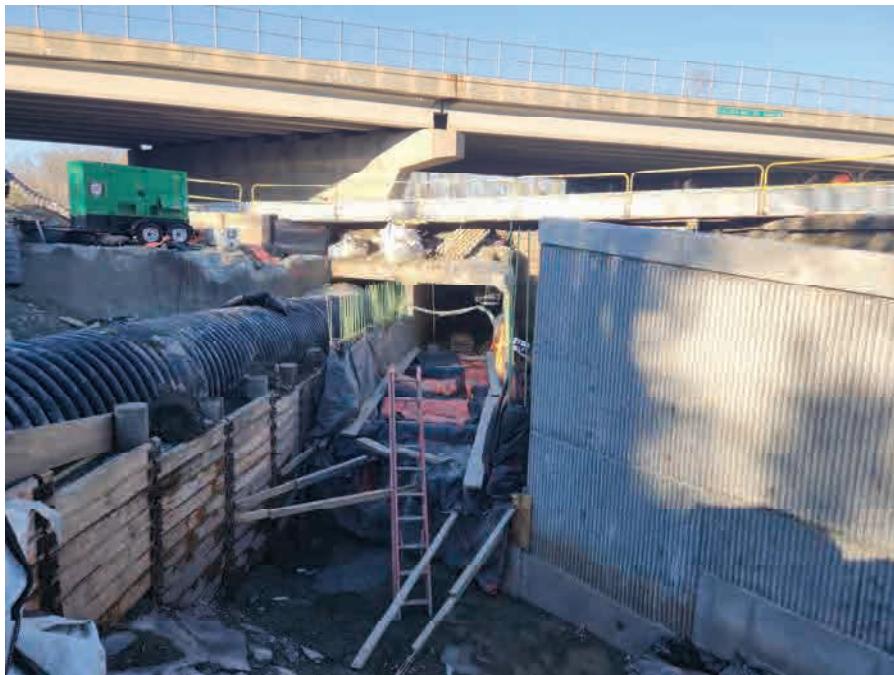


Bridge T-01-047 South Abutment

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-047 North Abutment



Bridge T-01-054 South Extension

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Utility Corridor along Route 140 South



Bridge T-01-047 South Abutment

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-046 Deck Formwork



Bridge T-01-045 South Abutment Footing

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-054 Roof Formwork



Bridge T-01-046 Deck Reinforcement

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-045 South Abutment



Concrete Placement at Bridge T-01-045 North Abutment Footing

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Beam Erection at Bridge T-01-047



Paving Roadway Widening on Route 24 North

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Overhead Sign Installation on Route 140



Bridge T-01-046 Concrete Deck Placement

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-054 North Extension Wingwall Footing



Bridge T-01-047 Formwork

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-045 North Abutment



Bridge T-01-045 Beam Erection

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402

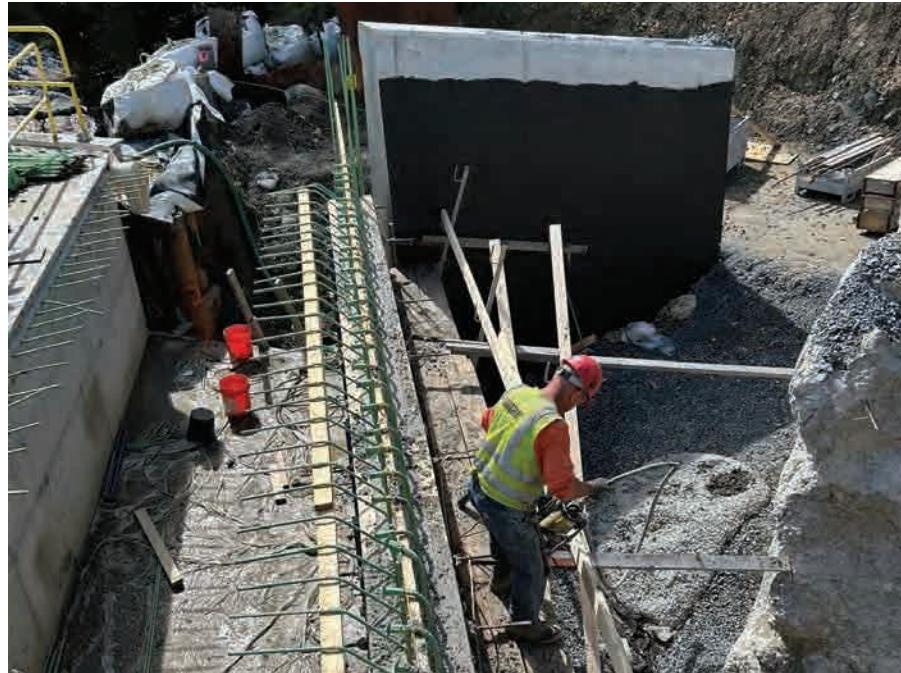


Bridge T-01-047 Approach Slab Concrete Placement



Bridge T-01-054 with Flow Diverted into East Barrel

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-054 West Barrel Construction



Bridge T-01-045 Concrete Deck Placement

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-046 Membrane Waterproofing and Snow Fence



Bridge T-01-047 Deck Pavement and Route 24 Widening

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402



Bridge T-01-054 Extension Completed and Flows through Barrels

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

Benesch Engineering, a highly respected and regarded engineering firm, provided a formal peer review of the project at the 100% Highway/Final Bridge design stage in June of 2020.

In addition, MassDOT's Highway, Bridge, Environmental, Landscape, Right-of-Way, and Traffic Divisions as well as appropriate District Departments (District Engineer, Utilities, Highways, Traffic) provided in-depth reviews of the 25% Highway/Sketch Plan, 75% Highway/First Bridge designs, and 100% Highway Second Bridge designs and its calculations book, Specifications, Special Provisions, and Cost Estimates as well as Construction Time Determination and Bottoms Up Estimates.

The OR reviewed the comments made during the above and the Project team's responses and attended comment resolution meetings held subsequent to these submissions. The OR also attended several coordination meetings held with the South Coast Rail project and kept notes of these discussions as the project progressed.

Value Engineering

A Value Engineering (VE) Study of the project design concept was conducted by McMahon Associates with nine possible cost-saving ideas identified. These were reviewed by the District Projects and Bridge Section and of the nine proposals, one was accepted; offering alternative precast retaining wall submittals while a second was conditionally accepted to allow a reduction to two lanes on Route 24.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues. However, it should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for its referral to the Cost Recovery Standing Committee. For any issue referred for Cost Recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 8/7/17) shall govern. In order to perform the legislative mandate of being the Primary Manager of Cost Recovery, the Owner's Representative will follow

the referenced SOP. This SOP, as modified for projects with an Owner's Representative, describes the role of the Owner's representative and includes a Cost Recovery Standing Committee. This committee oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary.

Extra Work Orders

As of November 30, 2025, there have been fourteen Approved Extra Work Orders:

1. Engineering cost related to the elimination of Real Time Traffic Monitoring
 - a. Disposition: approved by MassDOT & FHWA
 - b. Cost: \$14,650.68
2. Emergency Signal Repairs to the Route 140 at Route 24 Northbound Ramps
 - a. Disposition: approved by MassDOT & FHWA
 - b. Cost: \$3,772.47
3. Turbidity Monitoring related to updated SWPPP requirements.
 - a. Disposition: approved by MassDOT & FHWA
 - b. Cost: \$97,116.25
4. Plan Revisions for Wall 5 Height Increase.
 - a. Disposition: approved by MassDOT & FHWA
 - b. Cost: \$16,455.71
5. Lowering Rebar at Wall 5 Footing for Pipe Sleeve
 - a. Disposition: approved by MassDOT & FHWA
 - b. Cost: \$1,064.26
6. Turbidity Monitoring #2.
 - a. Disposition: approved by MassDOT & FHWA
 - b. Cost: \$99,891.00
7. Turbidity Monitoring #3.

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$50,000.00

8. Mobilization of Milling and Paving Operations.

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$27,891.14

9. Ramp F Loop Detectors for Interim Condition

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$3,576.84

10. Bridge 047 Contaminated Water Treatment System

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$122,329.60

11. Pedestal Disconnect Switches at Traffic Signal Locations

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$90,228.27

12. Bungalow Tie-in and Timing Changes at Ramp F

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$7,405.05

13. Turbidity Monitoring #4

- a. Disposition: approved by MassDOT & FHWA
- b. Cost: \$100,000.00

14. Trailing Anchorage Installation with the Use of a Vacuum Truck

- c. Disposition: approved by MassDOT & FHWA
- d. Cost: \$3,058.33

As of November 30, 2025, there is one Pending Extra Work Order:

1. Removal of Graffiti at Bridge 047
 - a. Disposition: approval is pending
 - b. Cost: \$10,475.67

As of November 30, 2025, there are two Probable Extra Work Orders:

1. Revisions to Traffic Signal Interconnect and Communications Cables
 - a. Disposition: in review by MassDOT
 - b. Cost: \$94,203
2. Additional Costs for Bridge 047 Contaminated Water Treatment System
 - a. Disposition: in review by MassDOT
 - b. Cost: \$50,000

As of November 30, 2025 there are four Extra Work Orders requested by MBI which have been denied by MassDOT:

1. Obstructions encountered when installing SOE at Bridge T-01-046. The Stage 1B portion of work related to this claim has been completed. This is currently in review.
2. Installation of debris netting under bridge T-01-046. This is stated in the project Special Provisions to be included in the payment for Item 994.02 Temporary Protective Shielding. A Step 1 meeting was held March 12, 2025. The next step is to hold a meeting with the Boston Claim Section.
3. Removal of asbestos weepholes at Bridge T-01-045. The project Special Provisions state that the demolition of bridge item includes the handling of all hazardous materials encountered in the demolition of the bridge. The demolition sub-contractor is preparing documentation requested in order to advance to a Step 1 meeting.

Project Name: Route 24/Route 140 Interchange Improvements, Taunton
Owner's Representative Contract Number: 120632
Project Number: 605888
Construction Contract Number: 112402

OWNER'S REPRESENTATIVE OATH

I Keith V. Lincoln, MA P.E. No. 42567, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120632, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



Keith V. Lincoln, P.E.

Date: December 16, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Reconstruction of I-90/I-495 Interchange, Hopkinton-Westborough

Owners Representative Contract Number: 80651

Project Number: 120638

Construction Contract Number: 116673



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative Contract No. 120638 – Jack Wright, Stony Brook CMS

Owner's Representative Work Order No. 2

Contract No. 116673 – Hopkinton-Westborough Reconstruction of I-90/I-495 Interchange

Design-Builder: Barletta Heavy Division (BHD)-O&G-Aetna I-495 I-90 JV

Preliminary Designer: HNTB

Prepared by: John J Wright, PE

December 12, 2025

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I. EXECUTIVE SUMMARY

Over the next four to five years this project will update and transform the I-90/I-495 Interchange. Due to electronic tolling and the subsequent elimination of the toll plazas, the interchange is now allowed to simplify the movements. The bridges need replacing and MassDOT is taking the opportunity to create a safer and more efficient interchange.

This very important and heavily used interchange must maintain all the movements throughout the rebuild. There are also several other challenges unique to this location.

The interchange sits in a very environmentally sensitive area. Marshlands abound and the Sudbury River flows through the site. The wetlands both abut the site and exist within the loop ramps.

The site also contains an archeologically sensitive area very close to the construction that must remain undisturbed.

Further complicating the site is the train line that crosses through the interchange. This is the mainline from Boston to Worcester and points west. The line carries Amtrak, Commuter Rail and freight trains. Coordination with the railroad will be a very important and substantial process.

II. ANNUAL REPORT

Project Scope of Work

The project will replace all ramps within this interchange. Due to space constraints caused primarily by the abutting wetlands, the new interchange is still required to be more complex than a standard full cloverleaf. However, the design does simplify most of the movements. Where possible, the design calls for direct connections that were not available when the interchange was built with a toll plaza.

The result of this undertaking will create safer roadway conditions and increased traffic flow that benefits business and the environment.

This project will require twelve (12) new bridges due to ramps crossing the mainline as well as bridges over the railroad and the Sudbury River. There are a variety of structures ranging from simple to complex.

Staging of the work is critical. Maintaining the movements between these two interstates while replacing the entire complex requires careful planning and execution. There are four major stages and one sub-stage.

MassDOT and their consultants and contractors are working closely with DEP on all the Environmental Issues. Such a large construction project happening in and around a significant wetland creates many concerns. As of this writing, the entire team is working well together to accomplish this task with as little disruption as possible.

There are environmental benefits included in the contract as well. Control of invasive plant species was one of the early activities undertaken.

Preliminary Design

The preliminary design scope by HNTB is complete except Construction Phase Services and assisting MassDOT with review of the final designs including:

- Permitting and environmental engineering.
- Structural engineering, civil engineering, electrical engineering, bridge architecture, utility engineering.
- Review plans and specifications.
- Review structural details.

Completed scope:

- Develop procurement documents for Design-Build process.
- Assist in procurement.
- Review of Final Design Packages

Construction (Design – Build)

The BHD-O&G-Aetna JV (BHD JV) team was procured using the Design-Build process. They are teamed with the design firm VHB. The work to be performed by BHD JV and their designers under this contract involves the final design and construction of the entire I-90/I-495 interchange. This scope includes:

- Advance the design from preliminary design through completion of final design – i.e., become the designer of record for all elements.
- Maintain traffic in accordance with traffic management plans.
- Design and Construction of 12 new bridge structures.
- New ramps for all eight moves in the interchange.
- Relocate or reconstruct existing utilities.
- Install new roadways with lighting, poles, and signage.

Major Progress as of December 1, 2025

- The Project has transitioned to Traffic Stage 2. It is worth noting that changing from stage to stage is an iterative process as movements are required at different locations and different times.
- Baseline schedule continues to be updated monthly followed by review meetings with the parties.
- Major work in the 495 median including earthwork and bridges have now allowed 495 Southbound traffic to be shifted onto what was the median. Demolition of the old Southbound bridges is complete and the new substructure for 495 over the RR is essentially complete. I-495 over I-90 encountered a substantial delay driven mostly by a settlement in the below grade soils that was not foreseen. As a result of this settlement, the MSE wall enclosing the bridge abutment needed to be partially demolished and is now being rebuilt after the soil was reinforced with timber piles. These issues prevented the steel erection for the new I-495 Southbound over I-90, thereby delaying the traffic shift of I-495 southbound onto its permanent new structure. This impact then ripples through the schedule hitting all remaining milestones.
- Fruit Street bridge structure is nearly complete, and traffic should be using both lanes by the end of the year. There remains roadway work on both approaches, which will be finished in the Spring with periodic lane takings.
- Ramp WN is finally rising above its major changes due to subsurface conditions. The trapezoidal box beams have been set, and the deck is taking shape. Deck pans are complete, and the studs are placed. Ironworkers are installing the deck rebar, and the contractor has submitted

a cold weather package looking to start deck placements this Winter. That plan is being reviewed for feasibility.

- Ramp ES was the first of the new Ramps to open to the public (November 2024).
- Ramp WS has steel erected and deck placed. The ramp has barriers up and is partially paved. Unfortunately, winter weather has arrived early this year and will not support finishing before Spring.
- I-90 over the RR has completed one section and is now working on the next.
- Design for Stages 3, 3B and 4 have been submitted and accepted. Design of all ramps is complete. Design of the structures is now only responding to field issues as they arrive.
- Whitehall Brook has completed its first segment and is on to Phase 2.
- Column and abutment work continues as available on Ramps SW and EN.

Budget

The initial budget (established in the contract encumbrance) was \$455,631,088 made up of:

- Design-Build Contract Price \$394,422,000
- Allowances \$17,266,877
- Original Contract Value \$411,688,877
- Contingency \$43,942,211
- Overruns and Underruns (\$6,369,683)
- Approved Contract Mods. \$30,231,621

○ Current Contract Value	\$435,550,816
○ Pending Contract Mods	\$11,884,138
○ Forecast at Completion	\$447,434,954

The total Amount earned by the Design-Builder is \$262,964,039.

This contract remains within the original financial forecast at present. There are still issues related to possible Differing Site Conditions that could impact that in the future. The District and the Field Office are working very hard to maintain budget.

Schedule / Project Milestones

- Notice to Proceed (NTP) Date = May 12, 2022
- Original Substantial Completion Date = June 24, 2027
- Original Final Completion Date = October 13, 2027

This contract is 59% complete based on paid values

- Contractor's Projection*
 - MS 6: May 5, 2023 (**A**) (Relocate Fiber Optic Cable)
 - MS 5: May 20, 2026 (**-597**) (Open new Ramp WN)
 - MS 4: May 19, 2028 (**-658**) Open new Ramp NE)
 - MS 3: May 27, 2028 (**-395**) Full Beneficial Use)
 - MS 2: April 19, 2029 (**-665**) (Substantial Completion)
 - MS 1: August 8, 2029 (**-665**) (Final Completion)

**These forecasts are based on BHD JV's latest Baseline Schedule*

Areas of Concern

The Design/Builder's design phase is complete but did not meet the baseline schedule assumptions. Coupled with the field work production the contractor impacted the milestones early. Also, much of the float in the original schedule has been used up. Subsequent to the Design Builder delays the Project began encountering a series of Differing Site Conditions (DSCs). These types of unforeseen changes are the responsibility of the Commonwealth according to Mass General laws (MGL). Complicating things further is the work is being performed almost entirely in or adjacent to environmentally sensitive areas. This is one of the largest contracts MassDOT has managed. Currently, the entire team is working well together. MassDOT, their Owner's Engineer (HNTB), the Engineer of Record or EOR (VHB) and the Contractor all have experienced and capable leaders. There is active communication with the abutters, businesses, municipalities, and legislators potentially impacted by the work. Environmental Agencies have also actively participated in the planning, design and construction oversight. A more positive note is that next Spring should see the opening of two important ramps. Ramp WS, I-90 westbound to I-495 southbound should be the next ramp to open. This ramp will eliminate the existing cross-weave at the old toll plaza site that is responsible for many accidents and extended traffic delays. Following in late Spring will be the opening of Ramp WN, I-90 westbound to I-495 northbound. This will also remove traffic from the old toll plaza and create a much simpler move for motorists. The benefits of these ramp openings should be very apparent to the travelling public.

Project Photographs



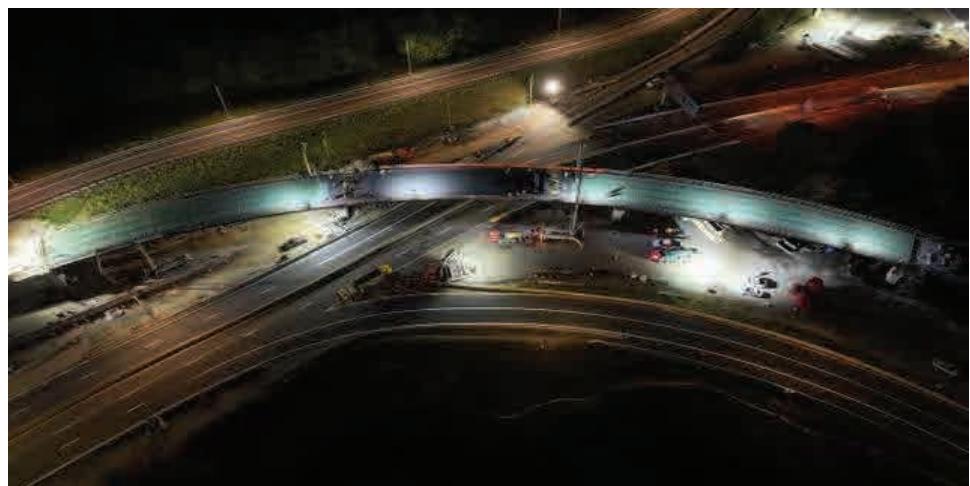
Ramp WS approach looking west



Drone view of Ramp WS towards south abutment



Ramp WS deck placement beginning at night





North end of Ramp WN showing end of trapezoidal box beam



Ramp WN with deck pans and studs installed and now rebar being placed



Timber piles being driven to improve ground at I-495 over I-90 north abutment



Timber piles completed and cut off at I-495 north abutment

III CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

During the final design/construction phase of this project, this O.R. is satisfying the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process to review and concur with any proposed design changes and their impacts to the project's quality, budget, or schedule goals. In addition, this O.R. also participates in the review process of the design submitted by the Design-Builder.

Value Engineering

MassDOT selected the Design-Build procurement method to gain efficiency in both cost and schedule. This method of procurement ensures Value Engineering principles are being constantly applied to the Project without the need for an independent report. An example would be the Contractor's Proposal which reduced the number of weekend closures to six from the allotted eight. By staying informed on the design progress and changes, the OR is able to participate in the informal VE process.

Cost Recovery

Some potential cost recovery issues are being looked at. Investigation is ongoing and no issues have been formally identified.

OWNER'S REPRESENTATIVE OATH

I John Joseph Wright, Sr., MA P.E. No. 43353, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120638, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.

A handwritten signature in blue ink, appearing to read "John Joseph Wright, Sr., P.E." The signature is fluid and cursive, with the "J" and "W" being particularly prominent.

John Joseph Wright, Sr., P.E.

Date: December 12, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip E. Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Corridor Improvements on Route 79/Davol Street

Owner's Representative Contract Number: 120632

Project Number: 608049

Construction Contract Number: 117658



Owner's Representative 2025 Annual Report

Owner's Representative Contract No. 120632 - Chappell Engineering Associates, LLC

Project No. 608049: FALL RIVER – Corridor Improvements on Route 79/Davol Street

Preliminary Design Consultant: TranSystems Corporation

Design-Builder: D.W. White, Inc. - SPS New England, Inc. Joint Venture

Engineer-of-Record: HNTB Corporation

Prepared by Chappell Engineering Associates, LLC

December 1, 2025

Project Name: Corridor Improvements on Route 79/Davol Street
Owner's Representative Contract Number: 120632
MassDOT Project Number: 608049
Construction Contract Number: 117658

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Project Name: Corridor Improvements on Route 79/Davol Street

Owner's Representative Contract Number: 120632

MassDOT Project Number: 608049

Construction Contract Number: 117658

I. EXECUTIVE SUMMARY

This project has reconstructed a 1.6-mile section of Route 79/Davol Street on the left bank of the Taunton River immediately to the north of Battleship Cove in Fall River. The overall objectives of the project are to re-establish connectivity between the neighborhoods and the waterfront, to promote economic growth by creating developable parcels, and to create a vibrant, multi-modal corridor. Prior to the start of the project Route 79 ran through the area as a grade-separated limited access highway with two northbound and two southbound traffic lanes and two-lane frontage roads (Davol Street). Route 79 has been relocated on to the two widened frontage roads thereby creating space for several development parcels in the former highway footprint. In addition to the reconstruction of President Ave. (Route 6) between Davol St. northbound and southbound, three new at-grade crossing streets have been constructed, Turner Street, Hathaway Street, and Brightman Street. Turner St. and Brightman St. are signalized at the intersections with Davol St. northbound and southbound, as is reconstructed President Avenue. Other project elements include shared use paths, sidewalks, ornamental lighting, extensive landscaping, a new bridge structure near the southern project limit, several retaining walls and two new CSO outfalls into the Taunton River.

In 2025 the Contractor has:

- completed installation of water and sewer mains and service connections, entirely new surface drainage and lighting systems, and six new traffic signal systems;
- completed installation of full-depth roadway, concrete curb and sidewalk, infiltration basins, and HMA shared use paths, project-wide;
- completed installation of buried conduit, handholes and manholes for Verizon, NGRID and Comcast project-wide;
- completed construction of the new 9'x 4' CSO box culvert from the project limit on President Ave. to the Remington Street outfall. Completed construction of the new 9'x 4' and 12'x 5' CSO box culvert from the project limit on Turner St. to the City Pier outfall.
- Installation of the extensive plantings is presently ongoing and close to complete.

To date in 2025 only one Extra Work Order has been approved (EWO#23, \$166,747: replacement of Comcast ductbank determined to have insufficient cover) and one is pending (EWO #24, \$140,000: design and installation of three stacked granite sign walls). By way of comparison, 2023 saw seven EWOs approved for a total of \$1,457,878 and 2024 saw ten approved for a total of \$1,439,310.

None of the EWOs received to date are considered candidates for cost recovery.

Project Name: Corridor Improvements on Route 79/Davol Street

Owner's Representative Contract Number: 120632

MassDOT Project Number: 608049

Construction Contract Number: 117658

At present, between pending and approved EWOs, materials price adjustments and projected contract item overruns/underruns, 56% of the original contingency monies has been committed (\$5,931,761 of \$10,695,682) while 90% of the estimated contract total has been invoiced (\$103,105,345 of \$114,730,188). As of 12/1/25, 77% of the contractual construction duration has elapsed (1116 of 1443 calendar days). The Full Beneficial Use milestone was certified by the District on 12/3/25, seven months before the contractual date of 7/4/26. As is clear from the information above, both from the point of view of project finances and of schedule, the project remains exceptionally well positioned.

II. ANNUAL REPORT

Contract Scope of Work

The project relocated Route 79 from its former alignment running down the middle of the 1.6-mile corridor on to the two frontage road alignments, thereby creating space for several development parcels within the current highway layout. Three new streets crossing the development parcels have been created, with at-grade signalized intersections, at Turner Street, Hathaway Street, and Brightman Street. The pre-existing crossing street, President Avenue, has been reconstructed. Other project elements include shared use paths, sidewalks, ornamental lighting, extensive landscaping, a complete new drainage system with two new outfalls into the Taunton River, and those elements the City of Fall River's CSO program within the project limits. Also included was the replacement of the bridge carrying Route 79 northbound only over the reconstructed U-turn near the southern project limit.

Major Progress as of December 1, 2025

All work is essentially complete. District certification of Full Beneficial Use milestone occurred on 12/3/25. Achievement of this milestone requires that “the majority of contract work has been completed and the asset has been opened for multi-modal transportation use except for limited contract work items that do not materially impair or hinder the intended public use of the transportation facility. All anticipated lane takings have been completed, except for minor, short term work items”. Previously, on 11/26/25, the Contractor issued a letter to the District Highway Director declaring the project to be Substantially Complete, however a requirement for Substantial Completion is that all materials submissions must have been received by the District materials lab, not yet the case.

Project Name: Corridor Improvements on Route 79/Davol Street

Owner's Representative Contract Number: 120632

MassDOT Project Number: 608049

Construction Contract Number: 117658

As noted above, to date in 2025 the Contractor completed corridor-wide installation of water and sewer mains and services, of a new surface drainage system and of a new lighting system. Also completed were six signalized intersections, full-depth roadways, concrete curb and sidewalk, infiltration basins, and HMA shared use paths, again corridor-wide. All infrastructure for subsurface utilities was replaced. The Contractor installed all buried conduit, handholes and manholes for Verizon, NGRID and Comcast. The utility companies have installed and connected their new lines. A subcontractor to Liberty Gas completed installation of new gas mains and services. The final sections of the new 9' x 4' box culvert from President Ave. to the Remington Street outfall completed, as did the final section of the new 12'x 5' and 9'x 4' CSO box culvert from Turner St. to the City Pier outfall. Due to City concerns expressed last year, the last of the design plans, landscape, did not complete until January, 2025. The extensive landscape plantings are close to completion, with final elements planned for the spring.

Budget and Project Financials

BUDGET:

	MassDOT	Municipal
Bid Price:	\$103,088,127	\$11,561,380
Allowances:	\$5,710,300	-
Traffic Police:	\$3,994,400	-
Traffic Police OT:	\$1,711,900	-
Trainees:	\$4,000	-
Original Contract Value:	\$108,798,427	\$11,561,380
Contingencies:	\$10,695,682	-
Original Encumbrance:	\$119,494,109	-

CURRENT FINANCIALS (as of 12/1/25)

	MassDOT	Municipal
Current Overruns/Underruns	\$2,934,174	(\$2,303,308)
Approved Contract Modifications	\$2,586,648	-
Current Contract Value	\$114,319,249	\$9,258,072

Project Name: Corridor Improvements on Route 79/Davol Street
Owner's Representative Contract Number: 120632
MassDOT Project Number: 608049
Construction Contract Number: 117658

Pending Contract Modifications	\$410,939	-
Estimated Contract Total	\$114,730,188	\$9,258,072
Probable Contract Modifications	\$0	-
Forecast Cost-at-Completion	\$114,730,188	\$9,258,072
Total CQE (Invoiced) to Date	\$103,105,345	\$8,495,781
Encumbrance Funds Remaining	\$7,892,963	-
Projected Funds Remaining	\$4,763,921	-

The City of Fall River is responsible for the funding of the following project elements:

- Upsizing of concrete box culverts for drainage in order to accommodate the increased flow rates from outside of the project limits as required for separation of sanitary and drainage systems (\$5.33 million);
- Underground storage chambers to replace surface infiltration basins at two locations (\$0.73 million);
- Upgraded “hardscape” including wayfinding signage, granite benches, bike bollards, uplighting under the bridge, etc. (\$1.28 million);
- Ornamental highway lighting (\$0.66 million);
- CCTV of City sewer lines (\$0.27 million);
- Construction of new sewer infrastructure within the project limits and designed by others as part of the ongoing separation of sanitary and drainage systems (\$3.28 million).

Schedule/ Project Milestones

The contract includes three milestones, as follows:

Milestone #3 – Full Beneficial Use

Defined as above: The majority of contract work has been completed and the asset has been opened for multi-modal transportation use except for limited contract work items that do not materially impair or hinder the intended public use of the transportation facility. All anticipated lane takings have been completed, except for minor, short term work items”. The milestone, contractually set as 1,332 calendar days from NTP, or 7/4/26, was certified complete by the District on 12/3/25, 214 calendar days before the contractual date.

Milestone #2 – Substantial Completion

Defined as follows: A walkthrough of the entire contract work performed by the Resident Engineer and a Punch List generated. Work associated with no more than one percent of the adjusted total contract price

Project Name: Corridor Improvements on Route 79/Davol Street

Owner's Representative Contract Number: 120632

MassDOT Project Number: 608049

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(including overruns, underruns and all contract amendments) yet to be completed. All materials submittals received by the District Materials Lab. The milestone is contractually set at 1,383 calendar days from NTP, or 8/24/26. The most recent schedule update (as of 11/10/25) shows the milestone occurring on 5/22/26, 94 calendar days before the contractual date.

Milestone #1 – Contract Completion

Defined as follows: All physical work complete, including Punch List, and Contractor has fully demobilized from field operations. Contractually set as 1,443 calendar days from NTP, or 10/23/26. The most recent schedule update shows the milestone occurring on 7/22/26, 94 calendar days before the contractual date.

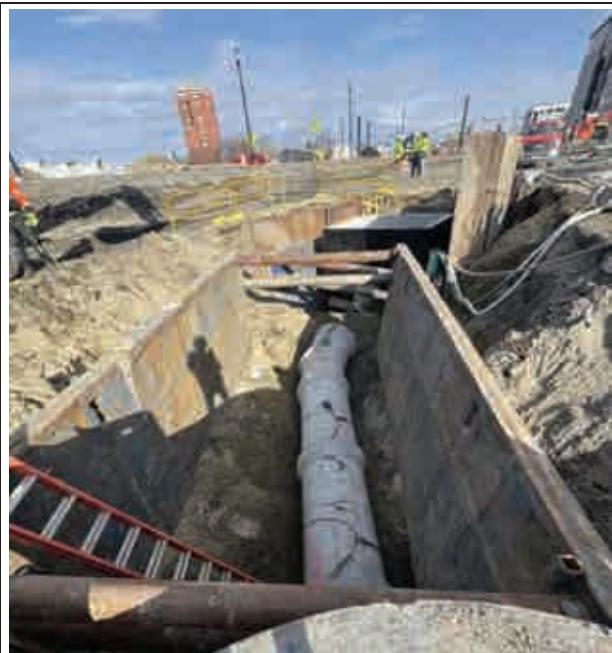
Areas and Issues of Concern

We have no issues of concern at this time. The project continues to carry positive float on all completion milestones. At this time several additional Extra Work Order requests are anticipated but not yet priced, however the added expense of the work orders should remain well within the original contingency budget. From the perspective of both finances and of schedule, the project remains exceptionally well positioned.

Progress Photographs



CSO regulator structure, local Davol NB
January 2025



CSO regulator, manhole & 36" RCP
January 2025

Project Name: Corridor Improvements on Route 79/Davol Street

Owner's Representative Contract Number: 120632

MassDOT Project Number: 608049

Construction Contract Number: 117658



Davol NB final alignment
April 2025



Looking south: BMP B under construction at Turner St.
April 2025



Looking south: Future Hathaway St. at Fall River Depot Station
June 2025



Looking north: New NB and SB shared use paths
June 2025



Turner St. with Davol NB signal operational
August 2025



Stamped concrete sidewalk under F-02-087
August 2025

Project Name: Corridor Improvements on Route 79/Davol Street

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New Davol NB alignment north of President Ave. August 2025	BMP A, retaining wall D-03 NW approach to F-02-087 August 2025
	
City Pier tide gate with accumulated tidal debris October 2025	Flooding on local Davol NB October 2025

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

During Procurement, reviews of the Preliminary Design Consultant's (PDC) 25% design documents were conducted by the concerned MassDOT sections and an independent "Bottom Up" cost estimate was performed by Patrick Engineering under contract to MassDOT. The Owner's Representative reviewed and commented on the draft RFP documents and addenda, attended the confidential Alternative Technical Concept (ATC) meetings and oral presentations with each proposer, and participated in meetings with the PDC to discuss the RFP documents and later, the ATCs and Technical Proposals. During Construction, the Owner's Representative continued oversight review of design and shop drawing submittals, RFIs and

Project Name: Corridor Improvements on Route 79/Davol Street

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NCRs, attended biweekly project meetings where construction issues, conflicts, and resolutions are discussed, and conducted regular site visits to observe field progress.

Value Engineering

A formal Value-Engineering study was not undertaken during the Procurement Phase, however development of the ATCs by the proposers serves a similar role in that the Design-Build teams are encouraged to develop improvements to the Base Technical Concept and present them to the Department for approval. While the D.W. White - SPS New England Joint Venture presented two ATCs which were subsequently accepted by the Department, once the project was awarded the Design-Builder elected to not pursue either.

Cost Recovery

No cost recovery issues have been identified to date.

Oath

I, Richard B. Littlefield P.E., MA P.E. No. 38773, hereby certify that my sole responsibility as Owner's Representative, under OR Contract No. 120632, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.



Richard B. Littlefield, P.E.

Date: December 1, 2025



Maurey Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Reconstruction of Route 20 from Richardson's Corner, Charlton to Route 12, Oxford

Owner's Representative Contract Number: 120632

Project Number: 602659

Construction Contract Number: 117832



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative Contract No. 120632 – Chappell Engineering Associates, LLC

Project No. 602659 – Reconstruction of Route 20 from Richardson's Corner, Charlton to Route 12, Oxford

CHARLTON-OXFORD, MASSACHUSETTS

Preliminary Designer: HDR Engineering, Incorporated

Design-Builder: D W White Construction Incorporated

Designer of Record: TranSystems Corporation

Prepared by: Chappell Engineering Associates, LLC

December 16, 2025

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I. EXECUTIVE SUMMARY

This project consists of improvements to the Route 20 Corridor in the Towns of Charlton and Oxford from the intersection of Route 20 with Richardson's Corner in Charlton to the intersection of Route 20 with Route 12 in the Town of Oxford. The project includes safety improvements to approximately 3.2 miles of Route 20, and includes the replacement of the bridges which carry Route 20 over the Little River in Charlton and the French River in Oxford. Safety improvements include the construction of a center median to separate east and westbound traffic, widening to allow for greater shoulder widths and turning lanes at intersections, traffic signal reconstruction, traffic calming, pedestrian and bicycle accommodations and additional roadside clearing to remove obstructions and allow for the creation of emergency pull off areas for police enforcement and vehicle refuge. A locus map of the study area is shown in Figure 1. This project is being constructed by the Massachusetts Department of Transportation (MassDOT) - Highway Division with HDR Engineering, Inc. as the Preliminary Designer of Record. The project was procured as a Design-Build Contract with Technical Proposals reviewed in August 2022 and Price Proposals opened August 31, 2022 with the apparent Best Value being the Design-Build Team of D W White Construction of Acushnet, MA as the Prime Contractor and TranSystems Corporation as Design-Build Designer of Record. Notice to Proceed was issued to D W White on November 17, 2022. Work on the project has progressed through the design phase through construction of the bridges, highway corridor and roundabout at Oxbow Road. The project reached substantial completion (full beneficial use) October 7, 2025.

The primary purpose of this project is to make safety improvements to the Route 20 Roadway within the project limits along with upgrading the roadway infrastructure and making environmental improvements to the resources located adjacent to the project. The intersection of Route 20 with Route 56 is a high crash location, in the top 5% of intersections and the corridor has a crash rate above the state average for roadways of this type in the state with 46% of these crashes being sideswipe, head on and single vehicle type which can typically be attributed to high travel speeds, narrow lane and shoulder widths, lack of separation between the east and westbound traffic and geometric deficiencies. Furthermore, there are no reasonable pedestrian or bicycle accommodations within the project limits which severely limits the mobility of any use of the corridor by any means other than a motor vehicle.

In order to address these safety and mobility concerns, the following improvements are proposed with this project:

- Traffic signal replacement and lane arrangements at the intersection of Route 20 and Route 56 specifically intended to address the safety concerns identified at the Road Safety Audit held in October 2014, report dated January 2015.
- Construction of a raised median or concrete barrier to separate the east and westbound directions of travel.
- Widening of the lanes and shoulders to allow for sufficient space between vehicles, drainage of the travel way and left turn lanes at the intersections.
- Improvements to the roadway horizontal and vertical alignment to provide greater sight distance and ability to remain in the travel lanes.
- Widening of Route 20 to provide a clear zone free from hazardous obstacles, emergency pull off areas and police parking enforcement areas to help control travel speeds.
- Installation of speed mitigation measures to assist in traffic calming.
- Construction of sidewalks and bicycle accommodations where feasible.
- Installation of guard rail in accordance with current requirements.

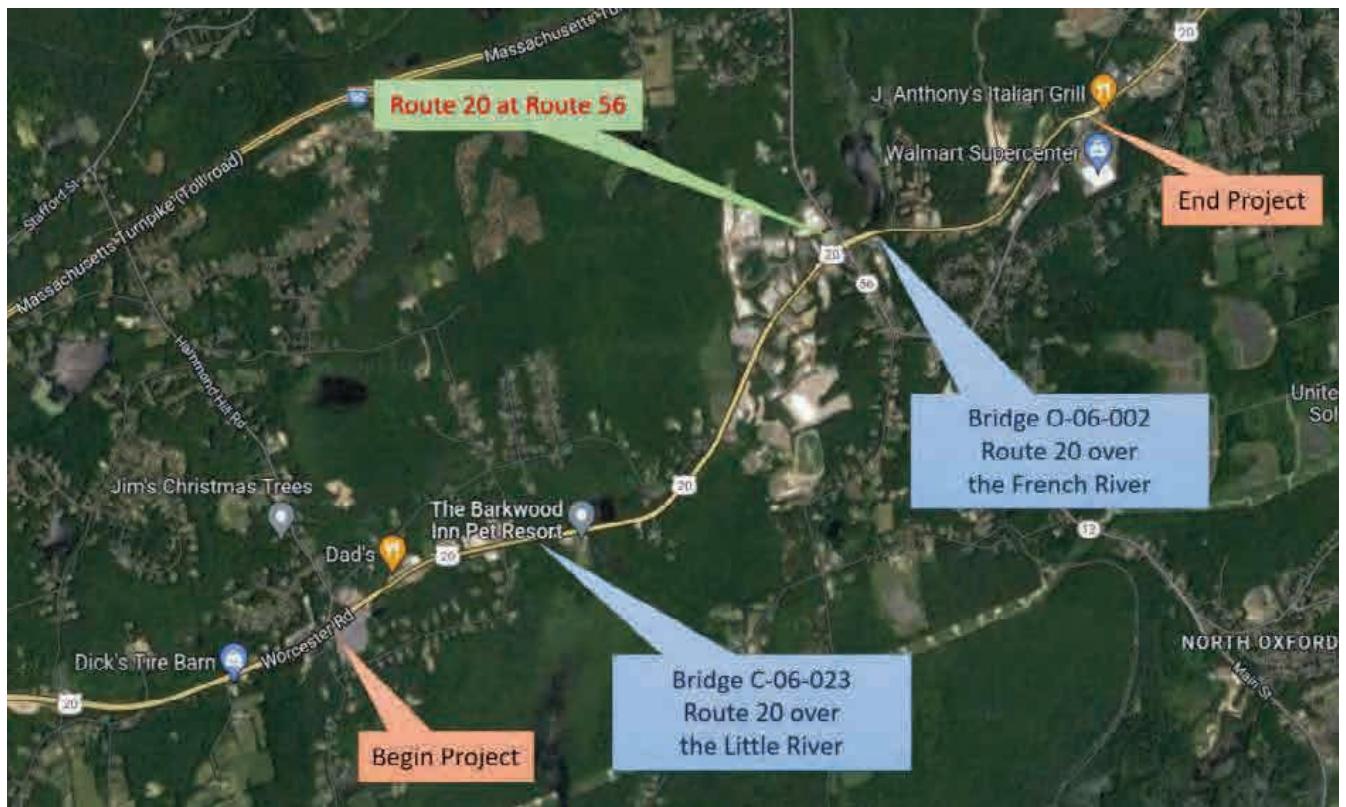


Figure 1 - Project Location

II. ANNUAL REPORT

Contract Scope of Work

Proposed improvements include:

- Reconstruction of the Route 20 corridor from the intersection of Route 20 with Richardson's Corner to the intersection of Route 20 with Route 12. Roadway reconstruction will be a combination of mill and overlay of the existing roadway with box widening. Full depth roadway reconstruction is proposed in areas where it is intended to correct the roadway cross slope to match the horizontal radius.
- Construction of a concrete barrier or median to separate the east and westbound travel ways on Route 20. A Minimum 2 foot left shoulder is proposed throughout the length of the project to provide separation of the roadway from the barrier.
- Installation of new fully actuated traffic signals at the intersection of Route 20 with Route 56 and for Route 20 at Richardson Corner Road/Oxbow Road. Both locations have existing signals which are intended for replacement. The intersection of Route 20 with Route 56 is a high accident location and improvements are intended to address very specific safety concerns raised during the Road Safety Audit held in October of 2014. Improvements at the Route 56 intersection will also include fully accessible improvements to facilitate pedestrian movements in accordance with the requirement of the Americans with Disabilities Act.
- Reconstruction of the Route 20 roadway will provide the opportunity to flatten the horizontal and vertical curvature to allow for the increasing of the available sight distance along the roadway. This will improve driver reaction time to consider avoidance maneuvers and avoid collisions with other vehicles as well as potential objects and pedestrians within the travel way.
- The Route 20 roadway is to be reconstructed with wider lane and shoulder widths to better allow for safe vehicular travel at the desired travel speeds. The increased roadway width is intended to allow for turning lanes at the signalized intersections with adequate left turn storage based upon updated traffic count information.

- The reconstruction is also intended to include speed mitigation measures to help prevent excessive speeding. This would include the construction of areas along the roadway where police can be posted and maintain a presence which will assist in the enforcement of speeds through the corridor. The project is also proposing speed feedback signage to raise driver awareness of excessive speeds. Advisory signage with speed reductions will be proposed in areas with roadway curvature or other potential features which require extra attention or speed reduction.
- Additional safety improvements will include the construction of new pedestrian and bicycle accommodations. The roadway within the project limits is constricted by the presence of wetland resource areas on either side of the road however an eight foot wide shared use path is proposed on the north side of Route 20 from the easterly intersection of the Oxbow Road with Route 20 to the easterly project limit of Route 20 with Route 12.
- Reconstruction will also include the creation of a roadside clear zone free of roadside hazards and to allow errant vehicles to recover and reenter the roadway. In areas where the roadside is restricted, guard rail will be proposed to redirect vehicles back toward the roadway and away from the non recoverable slope
- The full replacement of the two bridges carrying Route 20 traffic over the Little River (C-06-023) and French River (O-06-002). The new structures include full substructure and superstructure replacements consistent with the cross section of the approach roadways and compliant with environmental permitting and flood zones. The structures will meet current statutory loading requirements as well as seismic requirements and all applicable bridge design criteria.
- Extensive Stormwater Control Measures (SCM) are proposed throughout the corridor as required under the project permitting and as required by the state Stormwater Management Handbook prepared by the Massachusetts Department of Environmental Protection. The closed drainage system will be designed to provide greater resiliency to heavy storm events based upon current rainfall data and allow for better maintenance of the system as well as provide safety improvements by limiting roadway gutter spreads and water quality improvements by including measures to capture and treat stormwater through infiltration or through managed wetlands.

Major Progress As Of November 30, 2025

Price Proposals were publicly opened on August 21, 2022 with the Apparent Best Value being the Design-Build Team of D W White Construction of Acushnet, MA as the Prime Contractor and TranSystems Corporation as Design-Build Designer of Record. The following is a detailing of events which have transpired since Notice to Proceed was issued November 18, 2022:

- Weekly Project Coordination meetings began December 2022 and have continued through 2025.
- Environmental permit amendments have been prepared to accompany the project design have been submitted and reviewed in order to maintain the project schedule and milestones. Environmental monitoring has been on going throughout construction with erosion controls removed upon established site stability. The Towns of Charlton and Oxford Conservation Commissions have both signed off on project compliance.
- Public Information Meetings were held February 8, 2023, May 4, 2023. The February 2023 meeting was an opportunity to present the project and update the public on upcoming project activity. The May meeting was an opportunity to present more detailed information about the project including the change to the design where the proposed roundabout at Oxbow Road was displayed to the public. The meetings were well attended and well received by the public. The Design Build Team and MassDOT have maintained a consistent dialog in responding to questions posted after the meetings and in response to observed construction activity.
- A Public Information Meeting was held February 27, 2024. This meeting was effective in providing the public with a schedule of proposed activity through the life of the project. The meeting also provided the public with the opportunity to make the Design-Builder aware of current issues which were planned to be addressed in field meetings the following day.
- A Public Information Meeting was held March 25, 2025 to update the public on the planned construction activity through the upcoming season. The meeting was well received no major concerns raised. Comments and questions were taken into consideration and addressed to the maximum extent practicable.
- Overhead and underground utility relocations were completed project wide.

- Design work has been completed. Designer of record has supported construction throughout the work on the project.
- Construction of the Little River and French River Bridges has been completed along with wall construction throughout the project corridor.
- Completion of roadway widening, sidewalk construction and shared use path construction.
- Completion of traffic signal installations at the Richardson Corner and Route 56 intersections.
- Completion of highway lighting throughout the project.
- Completion of the Oxbow Road Roundabout.
- Final course paving, guard rail, signage and pavement markings installed throughout the project.
- Full beneficial use reached as of October 9, 2025 and project completion reached on November 20, 2025.
- Completion of wetland mitigation in accordance with environmental permitting.

Budget

- Office Estimate: \$61,883,270.04
- Price Proposal: \$95,584,000.00
- Original Encumbered Amount: \$108,399,549.80 (Includes Municipal Encumbrance)
- Current Encumbered Amount: \$109,799,549.80 (Includes Municipal Encumbrance)
- Total Amount Expended through November 30, 2025: \$ 99,390,384
- Current Estimate at Completion: \$109,799,549.80 (Includes Municipal Encumbrance)

The table below contains a summary of project financials through November 30, 2025:

Project Budget / Financials			
	<u>Contract</u>	<u>MassDOT</u>	<u>Municipal</u>
Bid	\$ 95,584,000	\$ 93,452,806	\$ 2,131,194
Allowances	\$ 3,469,000	\$ 3,469,000	\$ 0
Original Contract Value	\$ 99,053,000	\$ 96,921,806	\$ 2,131,194
Original Contingency (FIN681)		\$ 9,346,550	
Original Encumbrance		\$ 106,268,356	
Encumbrance Modifications		\$ 1,400,000	
Current Encumbrance		\$ 107,668,356	
Current Overruns and Underruns	\$ 465	\$ 465	\$ 0
Approved Contract Modifications	\$ 1,655,049	\$ 1,655,049	\$ 0
Current Contract Value	\$ 100,708,514	\$ 98,577,320	\$ 2,131,194
Pending Contract Modifications	\$ 5,482,900	\$ 5,482,900	\$ 0
Estimated Contract Total	\$ 106,191,413	\$ 104,060,219	\$ 2,131,194
Probable Contract Modifications		\$ 200,000	
Forecast Cost-at-Completion	\$ 106,391,413	\$ 104,260,219	\$ 2,131,194
Total CQE (Invoiced) to Date	\$ 101,521,578	\$ 99,390,384	\$ 2,131,194
Funds Remaining		\$ 6,146,778	
Projected Funds Remaining		\$ 3,408,137	
Expected Progress (baseline)	0.00%		
% Complete	95.42%		

Schedule / Project Milestones

- Notice to Proceed: November 17, 2022
- Completion of Little River Bridge: November 6, 2024
- Completion of French River Bridge: November 5, 2024
- Substantial Completion (Full Beneficial Use): October 7, 2025
- Final Completion: April 30, 2026

Areas of Concern

The following represents the primary areas of concern identified in design and at this stage in construction:

- Environmental Permitting: The project takes place within environmentally sensitive areas and continued coordination through final design and construction will need to respect these areas as well as permit conditions obtained in the preliminary design phase and amended conditions obtained in concert with final design. This has been mitigated as

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832

project work has been substantially completed and the Conservation Commissions of Charlton and Oxford have signed off on the project compliance.

- Travel speeds along the corridor remain a safety concern for the motoring public as it adjusts to the completed project travel ways. MassDOT will be undertaking a speed study in 2026 with the intent to determine new speed limit regulations throughout the corridor.

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
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Project Figures / Photographs



SCM Construction and Maintenance



Building Subgrade at Wetland Replication Area



Roundabout Construction



Raised Median Construction

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832



Roundabout Truck Apron Construction



SUP and Chain Link Fence Construction



Wetland Replication Area Construction



Roadway Paving on Route 20

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832



Guide Sign Installation



Shared Use Path Construction

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832



Placement of Approach Warning Strips to the Oxbow Roundabout



New Traffic Signal at Richardson Corner Intersection

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832



New Traffic Signal at Route 56 Intersection



Completed Wetland Replication Area

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832



Completed Route 20 Corridor in Charlton



Completed Route 20 Corridor in Oxford

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832



Completed Oxbow Road Roundabout



Ribbon Cutting

CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

A peer review was not performed prior to Design Build Procurement.

Value Engineering

A Value Engineering (VE) Study of the project design concept was conducted by The Jacobs Engineering Group in September of 2020 with thirty-eight (38) proposed alternatives with benefits in the form of safety, operations, environmental, construction and represented either design suggestions and/or cost savings alternatives. Of the 38 proposals, six were accepted, twenty-two were conditionally accepted and twelve were rejected. The Base Technical Concept and procurement documents have accounted for these in the preliminary design development.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues.

Extra Work Orders

As of November 30, 2025, there are eleven approved extra work orders:

1. Additional culvert replacement to be performed in the Town of Sturbridge. This had been previously planned under a separate contract which has been terminated. With the Termination of the contract it has been agreed to replace the culvert as an Extra Work Order under the Charlton Oxford (117832) Contract.
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$1,400,000.00
2. Installation of Temporary Portable Rumble Strips.
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$207,922.89
3. Asbestos Mitigation.

- a. Disposition: approved by MassDOT and FHWA
- b. Cost: \$47,125.71
4. Repairs to Existing Clogged Drainage System
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$35,361.87
5. Revisions to Stormwater Design Due to Stormwater Entering the Route 20 Roadway Drainage System from Off Site
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$125,481.60
6. Installation of Two New Drop Inlets and Relocation of Pipe and Headwall in Conjunction with the Eastland Partners Development in Progress Adjacent to the Project Site
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$181,603.13
7. Additional Paving and Installation of Wood Guard Rail at the Extra Space Storage Facility
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$19,976.12
8. Revisions Made to the Richardson Corner Traffic Signal
 - a. Disposition: approved by MassDOT and FHWA
 - b. Cost: \$16,234.06

Project Name: Route 20 Corridor Improvements, Charlton-Oxford
Owner's Representative Contract Number: 120632
Project Number: 602659
Construction Contract Number: 117832

As of November 30, 2025, there are two pending extra work orders:

1. Installation of Hard Wired Flashing Beacons at Roundabout
 - a. Disposition: approval is pending
 - b. Cost: \$49,357.59
2. Installation of Additional Granite Bounds
 - a. Disposition: approval is pending
 - b. Cost: \$109,885.16

As of November 30, 2025, there is one probable extra work order:

1. Installation of Preformed Warning Strips at Approach to Oxbow Roundabout
 - a. Disposition: in development by the Design-Builder
 - b. Cost: \$56,000

OWNER'S REPRESENTATIVE OATH

I Keith V. Lincoln, MA P.E. No. 42567, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120632, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



Keith V. Lincoln, P.E.

Date: December 16, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator

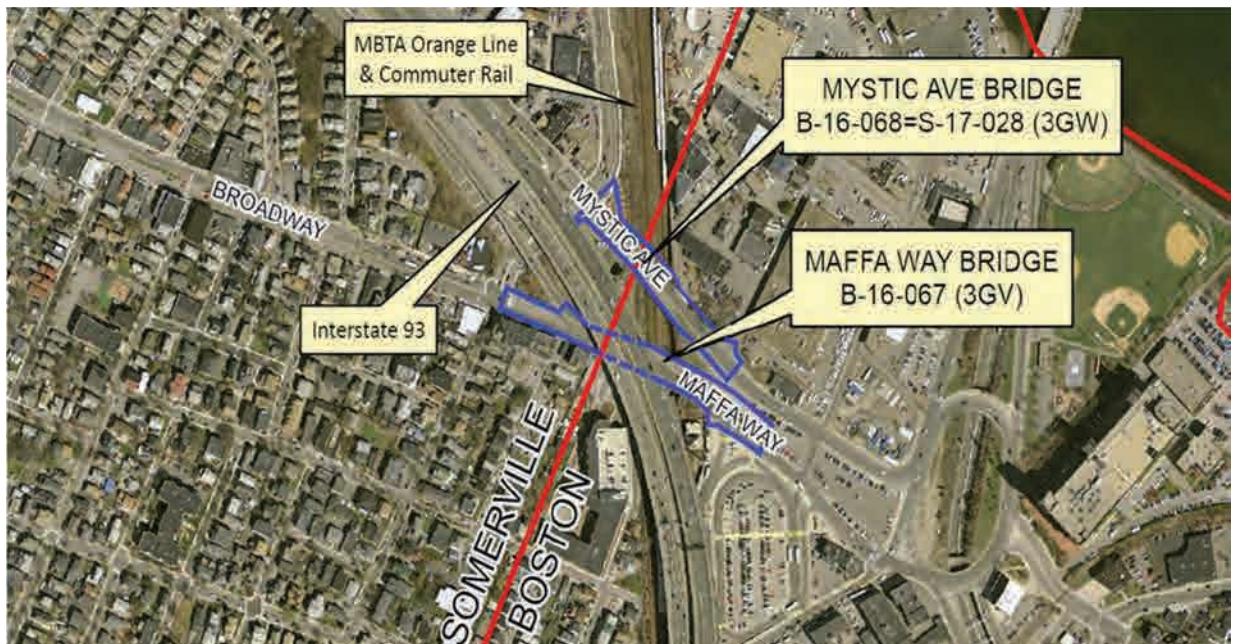


Project Name: Superstructure Replacement of Maffa Way & Mystic Avenue – Boston

Owner's Representative Contract Number: 120635

Project Number: 607670

Construction Contract Number: 119537



Owner's Representative's 2025 Annual Report

Owner's Representative Contract No.: 120635 JRP Consulting Services, LLC

Owner's Representative Work Order No.: 2

Project No. 607670 SUPERSTRUCTURE REPLACEMENT OF MAFFA WAY & MYSTIC AVENUE - Boston

Preliminary Designer: PARE Corporation

Design-Builder: Skanska USA Civil Northeast Inc. & CHA Consulting

Prepared by: Jeffrey Paul, P.E., DBIA

December 16, 2025

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I EXECUTIVE SUMMARY

The Maffa Way and Mystic Avenue Bridges were structurally deficient and in need of repair and rehabilitation. First constructed in the 1950's, the Maffa Way and Mystic Avenue Bridges carry approximately 30,000 and 27,000 vehicles daily, respectively, providing crucial transportation links between Somerville, the north shore, and Boston. When considering the extent of deterioration of the steel superstructures, the bridges' age, the location and amount of traffic volume they carry, and fatigue sensitive details, it was determined to replace the superstructures of both bridges.

To ensure the safety of these critical structures, MassDOT retained Pare Corporation to provide preliminary engineering services required to prepare construction bridging documents, the Base Technical Concept, to reconstruct and reconfigure the Maffa Way and Mystic Avenue bridge structures to a state of good repair. MassDOT chose to utilize a Best Value Design-Build Procurement process (BVDB) to accelerate delivery of the project. On February 22, 2023, the project was awarded to Skanska USA Civil Northeast Inc. with Lead Designer CHA Consulting. MassDOT is responsible for project advertisement, administering Federal funds associated with the project and construction oversight.

The work under this project consisted of the design and construction for the full removal and replacement of the existing bridge superstructures, the rehabilitation of the existing substructures to remain in place, moment slabs, and retaining walls. Roadway work included vertical and horizontal grade adjustments, full-depth pavement reconstruction, grading, granite curb, cement concrete sidewalks, dedicated bus and bicycle facilities, drainage, significant utility relocations, and other related work, including the reconstruction and resurfacing of a portion of the I-93 Southbound Off Ramp onto Maffa Way. The project includes extensive coordination with the MBTA, City of Boston, and City of Somerville.

The purpose of the project was to improve public safety and promote mobility for pedestrians, cyclists, public transportation, and vehicles. Project goals included: addressing structural

deficiencies by replacing two bridge superstructures with new code compliant and low-maintenance bridges; addressing traffic capacity and multimodal accessibility by maintaining general purpose travel lanes on each bridge, providing separate bicycle accommodations, improving sidewalks, and maintaining the existing bus lane on Maffa Way; and integrating with future projects including Rutherford Avenue improvements, One Mystic development, and completion of Assembly Square.

MassDOT and Skanska have committed to keeping the public informed as well as managing impacts as best they can while getting the project completed. Outreach efforts include: a project website, informational materials provided in community facilities, informational materials located on MBTA vehicles, public information meetings, Design Public Hearings, and briefings with local community organizations. Public Information Meetings were held in January 2024, May 2024, and February 2025.

To minimize impacts to MBTA track operations, the contract allows for two surge periods, one each in 2024 and 2025, consisting of nine consecutive days, where Orange Line operations would be shut down at Sullivan Station and replaced with bussing between Wellington and North Stations. The first surge was successfully completed early on Friday June 28, 2024, at 11am, thus meeting Incentive Payment #1 by 68 hours. This entitled Skanska to an incentive payment of \$897,600. The second surge was successfully completed early on Tuesday May 13, 2025, at 7AM, thus meeting Incentive Payment #2 by 139 hours. This entitled Skanska to an incentive payment of \$2,154,500.

As of November 30, 2025, the project is under construction. The Project contains a Contractual Notice to Proceed date of April 3, 2023 and a Contractual Contractor Field Completion date of December 30, 2026, resulting in a contract duration of 45 months or 3.7 years. The total project encumbrance is \$55,429,943 with a forecast cost at completion of \$74,626,512. The increase is largely due in part to the additional scope of work associated with the Lombardi/Broadway corridor improvements as well as MBTA bussing costs associated with the two nine-day Orange Line surges.

II ANNUAL REPORT

Contract Scope of Work

The Massachusetts Department of Transportation, Highway Division (MassDOT) proposed to replace the superstructures of the Maffa Way Bridge (B-16-067 (3GV)) and the Mystic Avenue Bridge (B-16-068=S-17-028 (3GW)) over the Orange Line, MBTA, and Pan Am Railways. The Maffa Way Bridge is located in the City of Boston, and the Mystic Avenue Bridge is located on the Boston/Somerville city line. The project included modifications to the approach roadways for each structure, including reconstruction of existing pedestrian accommodations and the addition of bicycle accommodations.

Built in the 1950's, the Maffa Way Bridge consists of a two (2) span continuous structure consisting of a reinforced concrete deck with a bituminous concrete wearing surface supported by rolled steel stringers. The superstructure is supported by reinforced concrete gravity wall abutments and one reinforced concrete wall pier supported on a spread footing. Maffa Way is classified as an urban principal arterial that runs east to west between Somerville/Boston City Lines and the Alford Street/Rutherford Avenue (Route 99)/ Main Street rotary. Additionally, this bridge is under MassDOT jurisdiction and is also part of the National Highway System (NHS).

The Maffa Way Bridge structure has an overall curb to curb width of 60 feet and an out-to-out width of approximately 83 feet. Within the curb-to-curb width, 50 feet is allotted for traffic from Broadway and the I-93 ramp combined while the remaining 10 feet on the northern is a crosshatched shoulder to dissuade vehicular use. The bridge has an existing 8.5-foot sidewalk with granite curbing on either side of the bridge.

Also built in the early 1950's, the Mystic Avenue Bridge consists of a three (3) span continuous structure consisting of a reinforced concrete deck with a bituminous wearing surface supported on rolled steel stringers. The superstructure is supported by a concrete

abutment on the east approach, a stone masonry abutment with a concrete beam seat on the west approach, and two reinforced concrete wall piers supported on spread footings. Mystic Avenue is a one-way urban principal arterial that runs east to west between Bailey Road in Somerville and Main Street in Boston, Massachusetts. It is under MassDOT jurisdiction and is also part of the National Highway System (NHS).

The Mystic Avenue Bridge has an overall curb to curb width varies from approximately 51-feet wide to 58-feet wide and the out-to-out width varies from 55-feet wide to 64-feet wide. Within the curb-to-curb width, at the start of the bridge 30-feet is allotted for motorist while the remainder is blocked off to prevent motorist from trying to enter using the parking lot to the southeast of the bridge. On the bridge the entire curb-to-curb width is used for motorists to navigate the roadway. The bridge has an 8.5-foot concrete sidewalk on the north side. There is no posted speed limit prior to or within the project area therefore a speed limit of 25 miles per hour is assumed, as both the cities of Boston and Somerville have opted into the state's policy regarding thickly settled or business districts.

When considering the extent of deterioration of the steel superstructures, the bridges' age, the location and amount of traffic volume they carry, and fatigue sensitive details, replacement of the superstructures proved to be a reasonable option. With both bridges nearing their expected service life, especially in a high-volume area, the existing superstructures would not be able to maintain a satisfactory level of service without major rehabilitation.

The scope of work included both design and construction for the full removal and replacement of the existing bridge superstructures, the rehabilitation of the existing substructures to remain in place, moment slabs, and retaining walls. Roadway work included vertical and horizontal grade adjustments, full-depth pavement reconstruction, grading, granite curb, cement concrete sidewalks, bicycle facilities, drainage, utilities, signage, traffic signal adjustments, pavement markings, and other related work, including the reconstruction and resurfacing of a portion of the I-93 Southbound Off Ramp onto Maffa Way.

The Project included significant utility relocations with multiple utility companies, coordination with the MBTA Orange Line and MBTA Commuter Rail/CSX Railroads for work within their railroad facilities, coordination with adjacent construction projects, and coordination with the Cities of Boston and Somerville.

A Best Value Design-Build Procurement process (BVDB) was utilized for the Project and consisted of a two-phase selection process. The first phase consisted of creating a short list of qualified Design-Builders and the second phase consisted of the submission of Technical and Price Proposals in response to a Request for Proposal (RFP). MassDOT established a Selection Committee approved by the Chief Engineer who was responsible for evaluating and ranking all Proposals on the basis of the evaluation criteria set forth in the RFP. In November 2022, the Design-Build team of Skanska USA/CHA Consulting was selected as the apparent winning team and on February 22, 2023 was subsequently awarded the contract.

Major Progress As Of November 30, 2025

As of November 30, 2025, the project is in construction.

Timeline and schedule of major events through November 30, 2025 include:

- Project awarded to Skanska USA/CHA Consulting on February 22, 2023
- City of Boston executed Right of Entry Agreement on March 29, 2023
- City of Boston executed Land Damage Agreement on March 29, 2023
- Construction Notice to Proceed (NTP) issued on April 3, 2023
- Preconstruction Meeting held on April 26, 2023
- Skanska/CHA held a Public Information Meeting on January 31, 2024
- Skanska/CHA executed force account agreements with MBTA, CSX, and Keolis in March 2024

- Skanska/CHA issued IFC Highway and Bridge Plans for Maffa Way and Mystic Avenue in March 2024
- Skanska/CHA commenced field activities in April 2024
- Skanska/CHA held Public Information Meetings on May 9, 2024 and May 13, 2024 in Charlestown and Somerville respectively
- Skanska completed first year 9-Day Surge Between June 22, 2024 and July 1, 2024, including removing girders over Orange Line Tracks, demolition of pier caps, erection of precast pier caps, and installation of steel girders over Orange Line and CSX tracks
- Skanska installed Phase 1 bridge deck on south side of Mystic Avenue and north side of Maffa Way
- Skanska/CHA held Public Information Meetings on February 24, 2025 and February 26, 2025 in Charlestown and Somerville respectively
- Skanska completed second year 9-Day Surge Between May 9, 2025 and May 13, 2025, including removing girders over Orange Line Tracks, demolition of pier caps, erection of precast pier caps, and installation of steel girders over Orange Line and CSX tracks
- Skanska installed Phase 2 bridge deck on north side of Mystic Avenue and south side of Maffa Way, thus completing both bridge decks
- Skanska commenced construction of the Lombardi/Broadway corridor improvements

On September 9, 2022 the MBTA approved the Memorandum of Agreement (MOA) between MassDOT and the MBTA. The MBTA approved the suggested construction schedule scenario that includes two nine-day Surges (Orange Line Diversions) for each calendar year of construction, with the first surge having been completed between June 22, 2024 and July 1, 2024 and the second Orange Line Surge completed between May 9, 2025 and May 13, 2025. The Surges consisted of nine consecutive days, beginning at 10:00 PM on Friday evening, with work occurring 24 hours per day until the end of the Surge at 5:00 AM on

Monday of the following week, where Orange Line operations were shut down at Sullivan Station. The MBTA agreed that bussing would be used to divert subway passengers between Wellington and North Stations. Coordination and scheduling of these Surges was the responsibility of the Design-Builder who worked closely with MassDOT Highway, MBTA, and the Cities of Boston and Somerville. The cost of bussing for the diversion will be borne by MassDOT Highway.

Budget

A project cost summary follows:

• Office Estimate:	\$34,120,416
• Bid Price:	\$39,200,000
• Encumbered Amount (MassDOT):	\$55,429,943
• Forecast Cost-at-completion:	\$74,626,512
• Anticipated Budget (Over)/Underrun-at-completion:	(\$19,196,569)
• Total Amount Expended through November 30, 2025:	\$ 45,668,878

The table below contains a summary of project financials through November 30, 2025.

Project Budget / Financials	Current	Month-to-Date	Month	Year
① Bid:	\$ 39,200,000	\$ 39,200,000	\$	\$
② Advances:	\$ 0,000,000	\$ 0,000,000	\$	\$
③ Original Contract Value:	\$ 45,668,878	\$ 45,668,878	\$	\$
④ Original Contingency (FWB&H):	\$ 0,000,000	\$ 0,000,000	\$	\$
⑤ Original Encumbrance:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑥ Encumbrance Modifications:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑦ Current Encumbrance:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑧ Current Overruns and Underruns:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑨ Approved Contract Modifications:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑩ Current Contract Value:	\$ 45,668,878	\$ 45,668,878	\$	\$
⑪ Pending Contract Modifications:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑫ Estimated Contract Total:	\$ 45,668,878	\$ 45,668,878	\$	\$
⑬ Postable Contract Modifications:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑭ Forecast Cost at Completion:	\$ 74,626,512	\$ 74,626,512	\$	\$
⑮ Total CQE (Invoiced) to Date:	\$ 45,668,878	\$ 45,668,878	\$	\$
⑯ Funds Remaining:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑰ Projected Funds Remaining:	\$ 0,000,000	\$ 0,000,000	\$	\$
⑱ Expected Progress (baseline):	\$ 0,00%	\$ 0,00%	\$	\$
⑲ % Complete:	\$ 100%	\$ 100%	\$	\$

Schedule/ Project Milestones

The latest Monthly Progress Schedule available is Update No. 28 which on November 21, 2025, was accepted by MassDOT.

The Project contains a Contractual Notice to Proceed date of April 3, 2023 and a Contractual Contractor Field Completion date of December 30, 2026. The Contract duration for the Maffa Way / Mystic Ave Bridge Replacement Project is approximately 45 months or 3.7 years.

The contractual Milestone 1 (Contractor Field Completion) date for the Maffa Way/Mystic Avenue Bridge Replacement Project is December 30, 2026. The forecasted Milestone 1 (Contractor Field Completion) date contained in the November 1, 2025 Monthly Progress Schedule is June 2, 2026 which is trending +211 calendar days ahead of schedule with no forecasted delays.

The contractual Milestone 2 (Substantial Completion) date for the Maffa Way/Mystic Avenue Bridge Replacement Project is August 25, 2026. The forecasted Milestone 2 (Substantial Completion) date contained in the November 1, 2025 Monthly Progress Schedule is December 10, 2025 which is trending +258 calendar days ahead of schedule with no forecasted delays.

The following is a list of contract milestones:

Milestone No. 6 – Release of Orange Line Reservation Area, Surge No. 1 by 1:00AM on the second Monday of Surge No. 1.

Milestone No. 5 – Restore Traffic on Maffa Way and Mystic Avenue, Surge No. 1 by 5:00 AM on the second Monday of Surge No. 1.

Milestone No. 4 – Release of Orange Line Reservation Area, Surge No. 2 by 1:00 AM on the second Monday of Surge No. 2.

Milestone No. 3 – Restore Traffic on Maffa Way and Mystic Avenue, Surge No. 2 by 5:00 AM on the second Monday of Surge No. 2.

Milestone No. 2 – Full Beneficial Use and Substantial Completion within 1,240 Calendar Days from NTP.

Milestone No. 1 – Design-Builder Field Completion within 1,367 Calendar Days from NTP.

Incentive Milestone Payments

Incentive Payment #1: Reduced Orange Line Surge Incentive

If the Design-Builder utilizes fewer than 219 hours for each of the two permissible Orange Line Closure and Diversions Surges, the Design-Builder will receive a performance Incentive Payment of Thirteen Thousand Two Hundred Dollars (\$13,200.00) per hour and a combined maximum Incentive payment of Three Million Six Hundred Thousand Dollars (\$3,600,000) when combined with Incentive Payment #2: Full Beneficial Use and Substantial Completion Incentive. Effective Friday June 28, 2024, at 11AM, Skanska achieved Milestone 06. The duration of the surge was 151 hours and ended 68 hours early, resulting in an incentive payment of \$897,600.

Incentive Payment #2: Full Beneficial Use and Substantial Completion Incentive

If the Design-Builder achieves Milestone No. 2: Full Beneficial Use and Substantial Completion prior to 1,240 calendar days from the Notice to Proceed, and in accordance with the work restrictions described, MassDOT shall pay an Incentive Payment of Fifteen Thousand Five Hundred Dollars (\$15,500.00) for each day Full Beneficial Use and

Substantial Completion is achieved prior to 1,240 calendar days from the Notice to Proceed. This Incentive is limited to a maximum Three Million Six Hundred Thousand Dollars (\$3,600,000.00) when combined with Incentive Payment #1: Reduced Orange Line Surge Incentive. Effective Tuesday May 13, 2025, at 7AM, Skanska achieved Milestone 04. The duration of the surge was 80 hours and ended 139 hours early, resulting in an incentive payment of \$2,154,500.

Disincentive Deductions

Disincentive Deduction #1: The Design-Builder shall be assessed a Disincentive Deduction based on calculated User Costs if they fail to release the MBTA Orange Line Reservation to the MBTA by 1:00 AM after interim Milestone No. 6 – Release of Orange Line Reservation Area, Surge No. 1 for the purpose of restoring normal service operations of the Orange Line. The Design-Builder shall be assessed a Disincentive Deduction of Five Hundred Sixty-One Thousand Dollars (\$561,000.00) per day beyond 219 hours until the Orange Line Reservation area is deemed ready for service by the MBTA. If the Design-Builder is assessed Disincentive Deduction #1, the Design-Builder will be prohibited from implementing the next Orange Line Closure and Diversion (Surge No. 2) until it has submitted, and MassDOT and MBTA have approved, an appropriate corrective action plan.

Disincentive Deduction #2: The Design-Builder shall be assessed a Disincentive Deduction based on calculated User Costs if they fail to release the MBTA Orange Line Reservation to the MBTA by 1:00 AM of the second Monday after interim Milestone No. 4 – Release of Orange Line Reservation Area, Surge No. 2 for the purpose of restoring normal service operations of the Orange Line. The Design-Builder shall be assessed a Disincentive Deduction of Five Hundred Sixty-One Thousand Dollars (\$561,000.00) per day beyond 219 hours until the Orange Line Reservation area is deemed ready for service by the MBTA.

Critical Path

The Maffa Way / Mystic Ave Superstructure Replacement Project Critical Path is driven by full depth roadway work, utilities, traffic signals, curbing, and sidewalks. The critical

path ends with Skanska installing landscaping and loam and seed to attain Milestone 1, Contractor Field Completion.

Areas and Issues of Concern

As of November 30, 2025 the contract is projected to be completed above the encumbered amount with additional funds being needed in the amount of \$19,196,569. This includes anticipated MBTA bussing cost associated with the 2025 9-day Orange Line surge (\$15,860,000), and costs associated with the incorporation of a dedicated bus lane on Lombardi Street, currently being tracked as PCO-004.

The following list represents potential areas of concern identified for the project that may impact the project budget and/or schedule:

- Increased MBTA costs associated with management, flagging, and bussing. The Memorandum of Agreement between MassDOT and the MBTA indicates a cost of \$3.6M. MBTA has noted that costs could be in excess of \$9M for such services for the 2024 Surge. MassDOT has requested documentation supporting MBTA's cost increase, however, as of November 30, 2025 MBTA has yet to produce such justification. On April 25, 2024 MBTA provided single line cost for bussing for May and June shutdowns. MBTA indicated MassDOT share of cost as \$8,067,000 which includes 100% share of May/June shutdowns and 50% share of May shutdown. Should increased costs for the aforementioned items be approved, the original project budget will be adversely impacted. It is anticipated that MBTA cost for the 2025 9-day Orange Surge is \$15,860,000 which is currently not accounted for in the project budget.
- Execution of Memorandum of Agreements (MOA) with the City of Boston and City of Somerville. Both Cities have expressed concerns with the BTC design and may not sign MOA's until their respective demands are met. The City of Boston raised concerns about Maintenance consisting of snow removal, de-icing, street sweeping, graffiti removal, landscaping, pavement marking and delineator maintenance, and trash removal for the bike lane and shared-use

sidewalks. The City of Boston has also expressed concerns with safety and usage. The City of Somerville is seeking a reduced speed limit of 25 mph (design speed utilized was 30 mph), narrower vehicle lanes (10'), an added bike lane on Maffa Way, and wider sidewalks. MassDOT and Pare have been proactively and extensively evaluating the feasibility of implementing the City of Boston and City of Somerville design requests. As of November 30, 2025, the City of Boston and City of Somerville have agreed to MassDOT's suggested alternative that implements intent of Cities design requests. Execution of the Memorandum of Agreements with both cities is still pending.

- Coordination with MBTA and allowable work windows associated with work on and over the tracks
- Inability to gain sufficient ROW access (to work from tracks or foul the clearance envelope) in a timely manner to support construction activities
- Providing accommodations for pedestrian, bicycle, transit, and vehicles (during construction). There is strong local advocacy for the first three as expressed at the Public Hearing and in subsequent letters to MassDOT
- Temporary traffic control requirements in the congested area surrounding the project
- Coordination with One Mystic Developer to accommodate anticipated concurrent work at the Northeast Quadrant of the Mystic Avenue bridge.
- Traffic management coordination with adjacent on-going construction projects (present and future) including Rutherford Avenue improvements, North Washington Street Bridge replacement, One Mystic development, and completion of Assembly Square.

The aforementioned issues will be monitored throughout the construction phase to mitigate potential impacts to project cost and construction schedule.

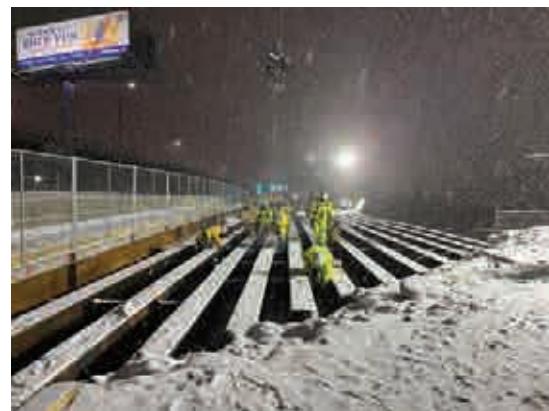
Project Photos



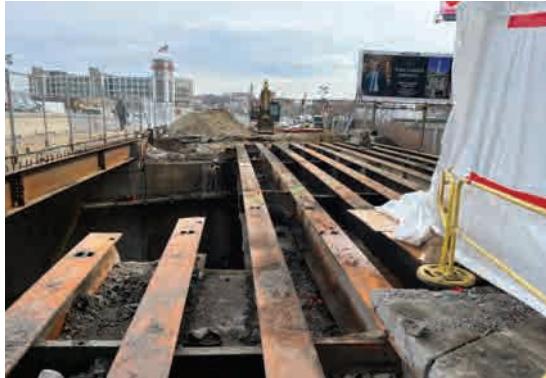
Aerial View of Project Location



February 2025 – Maffa Demolition



February 2025 – Maffa Deck Demolition



February 2025 – Maffa Deck Demolition



February 2025 – Mystic Deck Demolition



April 2025 – Maffa Way



June 2025 – Aerial View



June 2025 – Aerial View



July 2025 – Mystic Dry Run



September 2025 – Maffa Paving



November 2025 – Mystic Avenue

III CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

A Peer Review has not been conducted for this project, however, Patrick Engineering on behalf of MassDOT, prepared an independent CTD Schedule and Estimate. The CTD and Estimate were in close agreement with those prepared by City Point Partners and Pare Corporation.

During the construction phase of this project, this O.R. will satisfy peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

Value Engineering

A formal Value Engineering (VE) study has not yet been conducted for this project, however, MassDOT views the confidential ATC Process as a type of value engineering.

Cost Recovery

At this point in time, as construction has yet to commence, no Cost Recovery issues have been identified to date.

Approved/Pending/Probable Extra Work Orders (EWO's)

As of November 30, 2025, there have been eighteen (18) Approved Contract Modifications.

TOTAL APPROVED CONTRACT MODIFICATIONS = \$10,747,924.91

As of November 30, 2025, there are three (3) pending Contract Modification.

TOTAL PENDING CONTRACT MODIFICATIONS = \$4,054,892.05

As of November 30, 2025, there are three (3) probable EWOs. These include:

1. PCO 19 – Replacing MBTA Cables MH D979 to C29
 - a. Disposition: approval is pending
 - b. Cost: \$370,000.00
2. PCO 20 – Extending MBTA EPR Cables
 - a. Disposition: approval is pending
 - b. Cost: \$120,000.00
3. PCO XX – MBTA Phase 2 Bussing
 - a. Disposition: approval is pending
 - b. Cost: \$15,860,000.00

TOTAL PROBABLE CONTRACT MODIFICATIONS = \$16,350,000

Owner's Representative Oath

I, Jeffrey R. Paul, P.E., MA P.E. No. 38526, hereby certify that my sole responsibility as Owner's Representative, under OR Contract 120635, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.



Jeffrey R. Paul, P.E., DBIA

Owner's Representative

Date: December 16, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator

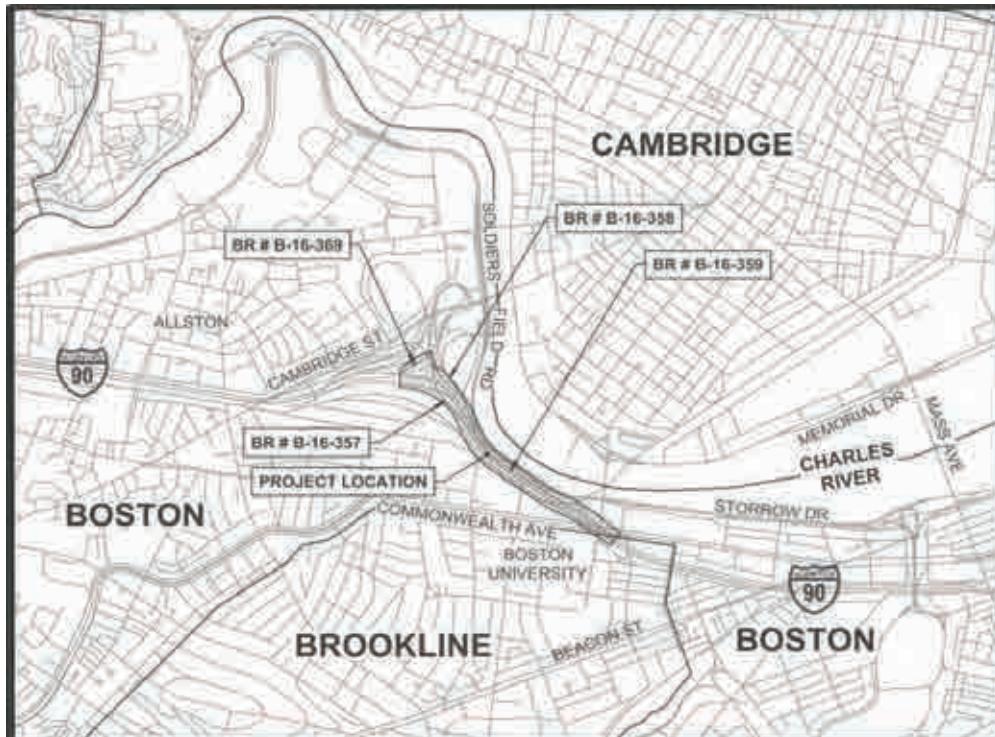


Project Name: Viaduct Bridge Repairs and Related Work (including Painting) Br. Nos. B-16-357, B-16-358, B-16-359, and B-16-369 Steel) along a Section of Interstate 90 (EB & WB) over the MBTA Railroad, Boston

Owners Representative Contract Number: 120638

Project Number: 612231

Construction Contract Number: 120260



OWNER'S REPRESENTATIVE 2024 ANNUAL REPORT

Owner's Representative Contract No. 120638 – Jack Wright, Stony Brook CMS

Contract No. 120260 – Boston I-90 over MBTA Bridge Preservation

Designer: WSP

Contractor: JF White

Prepared by: John J Wright, PE

December 12, 2025

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I. EXECUTIVE SUMMARY

The primary purpose of the project is to preserve the existing bridge structures to maintain the safe operation of the Commuter Rail and I-90 while MassDOT completes the design and environmental process for the larger, I-90 Allston Multimodal Project.

Interstate 90 is a vital East-West link between the City of Boston and the western suburbs. It is also a crucial link for freight from the Port of Boston to the rest of the United States. While a full replacement is being planned, this viaduct will have to remain in service for several years to come.

The structural rate of deterioration is exceeding normal maintenance activities. The scope of preservation work will prioritize only those elements necessary to preserve the structures for an additional 10-15 years. For example, the cleaning and painting of steel is focused on the beam ends and exterior beams. Some elements of work may help facilitate the larger Multimodal Project depending on final scope.

II. ANNUAL REPORT

Construction Scope of Work

- Bridge deck full and partial depth repairs
- Replacement of Bridge Deck Joints
- Repairs and localized replacement of concrete bridge barrier railings
- Replacement of Concrete Median Barrier with Portable Steel Barrier
- Selected Structural Steel Repairs and Painting
- Repairs to Concrete Abutments and Piers
- Rewiring and limited Repairs to Roadway Lighting System
- Cambridge Street bridge repairs

Design

- WSP is the Engineer of Record for the I-90 viaduct
- VHB is the Engineer of Record for the Cambridge Street bridge
- This is a traditional Design/Bid/Build Contract
- Production of Contract Documents
- Assistance in procurement.

Major Progress as of December 1, 2025

- Baseline schedule being updated monthly.
- Sounding for unbonded concrete both on the deck and on the structural columns below.
- Demolition and replacement of unsound concrete on the bridge deck.
- Demolition and replacement where necessary of concrete on substructure columns.
- Contractor is on call for urgent repairs to deck as the need arises.

Budget

The initial budget (established in the contract encumbrance) was \$85,733,503 made up of:

○ Contractor Bid Price	\$71,672,611
○ Allowances	\$3,310,000
○ Original Contract Value	\$74,982,611
○ Contingency	\$10,750,892
○ Current Contract Value	\$91,117,794
○ Pending Contract Mods	\$0
○ Forecast at Completion	\$91,117,794

The total Amount invoiced to date is \$36,071,834.

This contract has absorbed the Cambridge Street bridge work. That work was originally planned to be a separate contract. MassDOT chose to incorporate the work into this contract to avoid dealing with two contractors in the same space competing for lane takings and track time. The OR agrees this was a solid decision even though it will cause this contract to exceed its original budget. This is believed to be less expensive for the taxpayers overall.

Schedule / Project Milestones

- Notice to Proceed (NTP) Date = April 18, 2023
- Original Substantial Completion Date = October 7, 2026
- Original Final Completion Date = December 9, 2026

This contract is 40% complete based on paid values

- Contractor's Projection*
 - MS 3: October 2, 2027 (+39) Full Beneficial Use)
 - MS 2: October 2, 2027 (+39) (Substantial Completion)
 - MS 1: December 4, 2027 (+5) (Final Completion)

**These forecasts are based on JF White's latest Schedule Update*

The original completion dates have slid approximately one year due to the major scope addition of the Cambridge Street bridge.

Areas of Concern

Although the type of work is common to both MassDOT and the contractor, JF White, there are several factors which increase the challenge on the Project. Community involvement is extremely high on this project as the neighborhood and key abutters keep very close contact. The roadwork is on a very busy section of the I-90 Extension with both vertical and horizontal curves. There are also several other projects nearby. The railroad, which runs under, abutting and through the site adds complexity and coordination requirements.

This contract is designed to extend the life of the existing viaduct until the larger viaduct replacement project is underway.

A substantial portion of this contract can be performed under the viaduct without traffic impacts. There is work near/over the RR that requires coordination with the MBTA and flagging protection for the RR. There is also work that must be accessed through Boston University property and therefor requires coordination with BU. Work on the roadway deck is limited to night work with lane takings. These lane takings must be coordinated with other work in this area. During the Winter season this contract is almost completely shuttered except for responding to urgent deck repairs in coordination with MassDOT.

The length of the structure and the amount of deterioration make the scale of these repairs very large. The amount of traffic makes working on the deck very challenging. Safety of the workforce and the travelling passengers requires constant vigilance.

There is also the proximity to abutters. Harvard University, Boston University and Allston neighborhoods are very close and require communication and coordination. The MBTA Worcester Line and Grand Junction run through much of the site adding to the required coordination and safety vigilance.

Project Photographs



South side of I-90 Viaduct near beginning of project



South side of I-90 Viaduct in November 2025



2024 column repair with lower repaired and upper formed



2025 column repair completed



Typical conditions of concrete pier cap and steel beams prior to repairs (2023)



Typical conditions post repair work



Typical condition of twin structural steel girders at pier (2023)



Steel girders at pier post repairs



Major steel repairs to girders with temp shoring towers shown



Roadway joint being repaired on Ramp D (onramp to I-90 Eastbound)



Roadway joint repair completed on Ramp D

III CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

During the construction phase of this project, this OR is satisfying the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process to review and concur with any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

Value Engineering

The Owners Representative was assigned during the Procurement Phase of this Project. As this Project is bridge preservation, no formal Value Engineering was done. The work consists of practices that MassDOT and the District do constantly. However, the OR regularly discusses options for simplifying the scope with MassDOT managers. MassDOT is constantly looking for opportunities to make the work both efficient and helpful to the future build out of this interchange.

Cost Recovery

No cost recovery issues have been identified.

OWNER'S REPRESENTATIVE OATH

I John Joseph Wright, Sr., MA P.E. No. 43353, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120638, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.

A handwritten signature in blue ink, appearing to read "John Joseph Wright, Sr., P.E." The signature is fluid and cursive, with "John Joseph" on the top line, "Wright" on the middle line, and "Sr., P.E." on the bottom line.

John Joseph Wright, Sr., P.E.

Date: December 12, 2025



Maure Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owners Representative Contract Number: 80660

Project Number: 606783

Construction Contract Number: 120259



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

***Owner's Representative Contract No. 120632 – Chappell Engineering Associates, LLC
Project No. 606783 - NEWTON-WESTON BRIDGE BUNDLE, REPLACEMENT AND
REHABILITATION AT I-90/I-95 INTERCHANGE INCLUDING RAMP G (DB)
NEWTON AND WESTON, MASSACHUSETTS***

Preliminary Designer: Greenman Pedersen, Incorporated

Design-Builder: SPS-Walsh Joint Venture

Engineer of Record: Jacobs Engineering

Prepared by: Chappell Engineering Associates, LLC

December 16, 2025

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

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I. EXECUTIVE SUMMARY

This project consists of the reconstruction of the I-90 (Massachusetts Turnpike) corridor and adjacent ramps from approximately 700 feet west of the existing Park Road Bridge over Route I-90 in the Town of Weston to approximately 200 feet east of the existing Route I-90 Bridge over the MBTA/CSX Railroad in the adjacent City of Newton (See Figure 1). With this project, the bridges within these limits along with the associated roadways and ramps are to be reconstructed and realigned with revised lane configurations based upon future travel demands as well as anticipated travel speeds. The load carrying capacity of the new bridge structures will be increased to the current loading requirement for highway bridges (HL-93). The project will also strive to increase the clearance wherever possible of the proposed structures in an effort to obtain the minimum vertical clearance of 16'-6" and improve the roadway and shoulder widths to the desired values as recommended by AASHTO Standards.

The I-90 bridge over the Charles River and I-95 was in need of substantial rehabilitation in order to remain in service. This condition along with the adjacent project planned at the I-90 Allston Brighton interchange has created the need to execute and complete this project in advance of the Allston Brighton project.

In early November 2014, the preliminary design consultant, Greenman Pedersen Inc. (GPI) submitted an Alternatives Analysis study for the reconstruction of the I-90 over I-95 and Charles River Bridge which included a detailed breakdown of five separate alignment options. The study was reviewed by the District 6 Office and the Boston Office of MassDOT and a preferred option was selected. Notice to Proceed to advance the preferred option was given to GPI on December 23, 2015.

The preliminary design proceeded with the method of procurement established as Design-Build with Technical Proposals reviewed in February 2023 and Price Proposals opened March 1, 2023. The apparent Best Value was determined to be the Design-Build Joint Venture Team of SPS Construction with Walsh Construction as the Prime Contractor and Jacobs Engineering as Design-Build Designer of Record. Notice to Proceed was issued on April 22, 2023.

It has been agreed to include repairs to the existing wall supporting the embankment of the I-90 eastbound ramp (Ramp C) to I-95 southbound which had been identified as deficient by the Geotechnical Section of MassDOT. In preparing the proposed wall designs for the Soil Nail Wall 1 MSE Wall 2 intended to replace the existing BIN Wall adjacent to Ramp C, deterioration has

been observed on Bridge W-29-032 which supports the Ramp C Roadway above the Hultman Aqueduct. The Design-Builder has raised the concern that this deterioration may compromise the integrity of the replacement walls. It has been determined that a full replacement will be necessary due to safety concerns associated with repairs to the structure.

It has also been determined to include combined bridge (Bridge N-12-065 = W-29-035) replacements for the I-95 northbound ramp to I-90 east and west (Ramp A) and I-90 east/west to I-95 northbound (Ramp B). Plans for the Ramp A/B bridges were developed to final design however a contract had not been executed and it was decided to include these bridge replacements in this contract to take advantage of the mobilized contractor. In releasing the Design-Build Procurement Documents, the final design plans for the Ramp A/B Bridges were revised to conform to the preliminary format of the Base Technical Concept Plans to allow for innovation and design development in the Design-Build process. The Design-Builder has informed MassDOT that substructure reuse for Ramp A/B over the Charles River (N-12-065) as proposed in the Base Technical Concept is not feasible. After consideration of multiple alternatives, the Design-Builder has concluded that the only feasible alternative for a full structure replacement is a three span structure. The Design Builder has indicated that this work can be accomplished within the existing contract milestones. Issued For Construction drawings have been approved and MassDOT has directed the Design-Builder to procure long lead time items.

In response to comments issued during the Public Information Meeting held March 31, 2022, it has been determined to also include the repairs identified in the inspection of the Ramp A/B Bridge over the Charles River and I-95 (N-12-064=W-29-034). In performing the demolition of the existing N-12-064 it has been determined that the deterioration of the existing bridge deck is so extensive that the scope is being re-evaluated to consider a full bridge deck replacement in lieu of targeted deck repairs.

Design work on the project has progressed with Issued for Construction packages being accepted for all of the bridges on the project as well as the Highway Plans for the project.

Construction work currently in Stage 2A including three eastbound lanes of travel on the southerly off line portion of the I-90 over I-95 and the Charles River Bridge. The existing eastbound portion of this bridge is in demolition in anticipation construction of the future temporary westbound roadway of the bridge constructed in 2026. Construction has also been

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

completed at the I-90 over the MBTA/CSX Bridge with superstructure successfully performed over two MBTA weekend shutdowns in the spring of 2025.

The off line portions of the Ramp G Bridges are in construction with the intent to complete these bridges and move traffic onto these bridges in 2026.



Figure 1 - Project Location

II. ANNUAL REPORT

Contract Scope of Work

The scope of work includes but is not limited to the reconstruction of the I-90 roadway from approximately 700 feet west of the existing Park Road Bridge over Route I-90 in the Town of Weston to approximately 200 feet east of the existing Route I-90 Bridge over the MBTA/CSX Railroad in the adjacent City of Newton and the associated on and off ramps between the communities of Newton and Weston.

In addition to replacing the Route I-90 over I-95 and Charles River Bridge (Bridge N-12-066 = W-29-036), this project proposes complete replacement of the Ramp G Bridge over Ramps A and B (Bridge W-29-058); Ramp G Bridge over existing MWRA Aqueduct (Bridge W-29-057); Ramp G Bridge over Route I-90 (Bridge W-29-055). The superstructure of the Route I-90 over MBTA Railroad (Bridge N-12-067) will be replaced as well as modifications to the substructure. Minor modifications to the Route I-90 bridge over Charles Street (Bridge N-12-073) are also proposed to accommodate the widening of I-90 over the existing structure.

Additional elements involved in the design include but are not limited to: drainage improvements; mechanically stabilized earth (MSE) retaining walls, expanded polystyrene backfill rock fill; concrete median barrier; highway lighting; stormwater improvements: overhead sign structures; full depth hot mix asphalt roadway construction; micro-milling with hot mix asphalt overlay; paving markings and signage; utility relocations; sound barrier walls and temporary traffic control.

It has been decided to include the replacement of the Ramp A/Ramp B bridge over the Charles River (N-12-065 = W-29-035) in this contract. This bridge combines the ramps providing the connection of I-95 north and south with I-90 east and west. Plans for this replacement had been prepared all the way through the PS&E stage however a contract was not executed. In addition, repairs to the wall supporting the embankment of the I-90 eastbound ramp (Ramp C) to I-95 southbound which had been identified as deficient by the Geotechnical Section will be included with the improvements to be made with this project. It has also been agreed to include suggested repairs to the Ramp A/Ramp B bridge over the Charles River and I-95 (N-12-064 = W-29-034). The repairs include parapet replacement, partial deck reconstruction, median replacement, repair to beam ends and bearing stiffeners the painting of the bridge and pier repair.

Major Progress As Of November 30, 2025

The following is a detailing of events which have transpired in 2025:

- Weekly Project Coordination meetings, initiated in June 2023, have continued throughout the entire year.
- Issued for Construction (IFC) design packages were issued for the bridges of I-90 over I-95 and the Charles River (N-12-066 = W-29-036), I-90 over the MBTA (N-12-067), Ramp G over I-90 and Ramp K (W-29-055), Ramp G over the MWRA (W-29-057) and Ramp G over Ramps A-B (W-29-058), Ramps A/B over I-95 and the Charles River (N-12-064=W-29-034), Ramps A/B over the Charles River (N-12-065 = W-29-035) and the Highway Design Package.
- Environmental permit and MWRA permit amendments have been prepared to accompany the project design and continue to be monitored in accordance with permit conditions in order to maintain the project schedule and milestones.
- The Design-Builder completed construction of the trestle across the Charles River to provide access to the work zones required to construct the I-90 over I-95 and the Charles River Bridge.
- A Public Information Meeting was held April 25, 2024. An overview of the entire project was presented along with specific details of planned construction work.
- Public Information Meetings were held April 15 and April 17, 2025. These meetings provided the public with an update of current project activity and upcoming project work with an emphasis on the upcoming weekend closure work planned at the MBTA Railroad Bridge (N-12-067). A very direct message was delivered to the motoring public to try to avoid using I-90 during these weekends if at all possible and if necessary to expect significant delays. The Design Builder has constructed the Stage I portion of Piers 1, 2 and 3 for Bridge N-12-066 = W-29-036.
- Completion of construction of the east and west abutments for Bridge N-12-066 = W-29-036 and approach Walls 17 and 20.

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

- Completion of Stage I portion of Bridge N-12-066 = W-29-036 including installation of girders, placement of reinforcement and concrete deck, paving the deck and construction of approach roadways. Upon completion, eastbound I-90 traffic has been diverted to the newly constructed bridge.
- Begin demolition of Stage II portion of the existing N-12-066 = W-29-036 bridge including abutments and superstructure.
- Begin construction of accessible portions of substructure for Bridge N-12-066 = W-29-036.
- Successful completion of construction of the superstructure replacement Bridge N-12-067. Work included two weekend shutdowns of the MBTA Commuter Rail and reducing the I-90 roadway to one lane in each direction. Each weekend shutdown was completed in accordance with contract requirements and the MBTA and I-90 travel ways were opened to the public as stipulated at the conclusion of each weekend. Upon completion of weekend closure, bridge decks were subsequently paved and striped in permanent configuration.
- Begin construction of Ramp G Bridge over the Hultman Aqueduct (W-29-057). Work included installation of piles, abutment construction, installation of girders, placement of reinforcement and concrete deck, approach roadway and mechanically stabilized walls.
- Begin construction of Ramp G over I-90 and Ramp K (W-29-055). Work included installation of piles, abutment and pier construction and partial installation of girders.
- Begin construction of Ramp G over Ramps A-B (W-29-058). Work included installation of piles, abutment and pier construction and approach roadway construction.
- Fiber Optic Cable relocations continue project wide as construction staging requires.
- Highway construction work proceeds project wide including construction of approach roadways, drainage installation and highway lighting.

Budget

- Office Estimate: \$288,102,180

- Price Proposal: \$318,831,313
- Encumbered Amount: \$386,681,064
- Current Encumbered Amount: \$386,681,064
- Total Amount Expended through November 30, 2025: \$185,370,345
- Probable Contract Modifications: \$101,035,844
- Current Estimate at Completion: \$442,925,698

The table below contains a summary of project financials through November 30, 2025:

	Contract	MassDOT	Municipal
1 Bid	\$ 318,831,313	\$ 318,831,313	\$ 0
2 Allowances	\$ 20,025,054	\$ 20,025,054	\$ 0
3 Original Contract Value	\$ 338,856,367	\$ 338,856,367	\$ 0
4 Original Contingency (FIN801)		\$ 47,824,897	
5 Original Encumbrance		\$ 386,681,064	
6 Encumbrance Modifications		\$ 0	
7 Current Encumbrance		\$ 386,681,064	
8 Current Overage and Underage	\$ (888,025)	\$ (888,025)	\$ 0
9 Approved Contract Modifications	\$ 3,692,151	\$ 3,692,151	\$ 0
10 Current Contract Value	\$ 341,889,854	\$ 341,889,854	\$ 0
11 Pending Contract Modifications	\$ 26,381	\$ 26,381	\$ 0
12 Estimated Contract Total	\$ 341,889,854	\$ 341,889,854	\$ 0
13 Probable Contract Modifications		\$ 101,035,844	
14 Forecast Cost-at-Completion	\$ 442,925,698	\$ 442,925,698	\$ 0
15 Total CGE (Invoiced) to Date	\$ 185,370,345	\$ 185,370,345	\$ 0
16 Funds Remaining		\$ 201,310,718	
17 Projected Funds Remaining		\$ (56,244,034)	
18 Expected Progress (baseline)	0.00%		
19 % Complete	41.85%		

Schedule / Project Milestones

- Milestone No. 4: Completion of the Weekend 1 Superstructure Replacement at N-12-067: June 2, 2025 was successfully met.
- Milestone No. 3: Completion of the Weekend 2 Superstructure Replacement at N-12-067: June 8, 2025 was successfully met.
- Milestone No. 2 – Full Beneficial Use/Substantial Completion: October 28, 2028

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

- Milestone No. 1 – Design-Builder Field Completion: November 27, 2028

The most recent update of the Design-Builder's proposed schedule respects the current contract milestones.

Areas of Concern

The following represents the primary areas of concern identified in design and in the preliminary construction:

- In a meeting at the site on August 29, 2019 with MassDOT District 6 personnel, GPI and Algonquin Gas, the project's potential impact on the 24 inch high pressure gas line serving Boston and Cambridge was discussed. The line goes under I-95 where it is sleeved, most likely with a larger diameter steel pipe and has limited redundancy that would not likely allow it to be out of service. Algonquin provided coordinates to pinpoint the location of the line which very closely matched the location shown on the GPI design plans. It has been agreed that this situation will be monitored to ensure that there will be no impacts to the utility and that the necessary construction precautions are taken as the construction proceeds.
- There is a fiber optic cable line on the north and south sides of I-90 which will need to be relocated with the proposed design and construction as the project proceeds. The Design Builder has coordinated this work with the District Utilities Engineer and work to install new conduits and manholes continues in combination with project staging and progress.
- The Hultman Aqueduct (MWRA utility) which travels through the project site has two overpasses and will need to be considered throughout the design. The Design-Builder has submitted amended permit applications through the MWRA in coordination with current design. Protective slabs were installed over the pipeline as required to avoid damage caused by heavy loading. This will need to continue as work proceeds to protect this infrastructure.
- Construction staging and maintenance of traffic continues to be a very challenging element of the project due to heavy traffic volumes and numerous bridge replacements within the project limits requiring careful staged construction. The contract documents stipulate that three lanes of travel both east and westbound must be maintained during peak travel hours of construction.
- The Design-Builder has informed MassDOT that substructure reuse for Ramp A/B over the Charles River (N-12-065) as proposed in the Base Technical Concept is not feasible. After consideration of multiple alternatives, the Design-Builder has concluded that the

only feasible alternative for a full structure replacement is a three span structure. The Design-Builder has prepared a memorandum documenting the evaluation of the alternatives with a recommendation for a three span replacement structure. An Extra Work Order has been submitted for this change resulting in an increase in the project cost of 72 million dollars to the contract. This Extra Work Order remains in negotiations. The Design-Builder has indicated that this work can be accomplished within the existing contract milestones. MassDOT is reviewing this memorandum and has directed the Design-Builder to advance the design with a full substructure replacement. IFC plans for the full bridge replacement have been accepted and the Design Builder has been directed to procure long lead items while negotiations are in process for this Extra Work Order. Additional funds will need to be encumbered to account for this additional work.

- In preparing the proposed wall designs for the wall intended to replace the existing BIN Wall supporting the embankment adjacent to Ramp C, deterioration has been observed on Bridge W-29-032 which supports the Ramp C Roadway above the Hultman Aqueduct. The Design-Builder has raised the concern that this deterioration may compromise the integrity of the replacement walls. A field meeting was held to evaluate the deterioration and investigate potential repairs to be made to the existing bridge. It has been determined that a full replacement will be necessary due to safety concerns associated with repairs to the structure. The Design-Builder to will need to comment on feasibility of construction and provide an estimated construction cost.
- In performing the demolition of the existing N-12-064 it has been determined that the deterioration of the existing bridge deck is so extensive that the scope is being re-evaluated to consider a full bridge deck replacement in lieu of targeted deck repairs. A request has not been submitted or reviewed however an estimated order of magnitude cost of this additional work is 29 million dollars. Final scope determination and pricing remains to be submitted by the Design-Builder.

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

Project Figures / Photographs



Begin Girder Installation across Charles River (Bridge N-12-066)



West Abutment and Wall 17 (Bridge N-12-066)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



East Abutment Piles (Bridge N-12-066)



Girder Installation across I-95 (Bridge N-12-066)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Support of Excavation along Ramp G



Girder Installation at East Abutment (Bridge N-12-066)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Formwork and Deck Reinforcement (Bridge N-12-066)



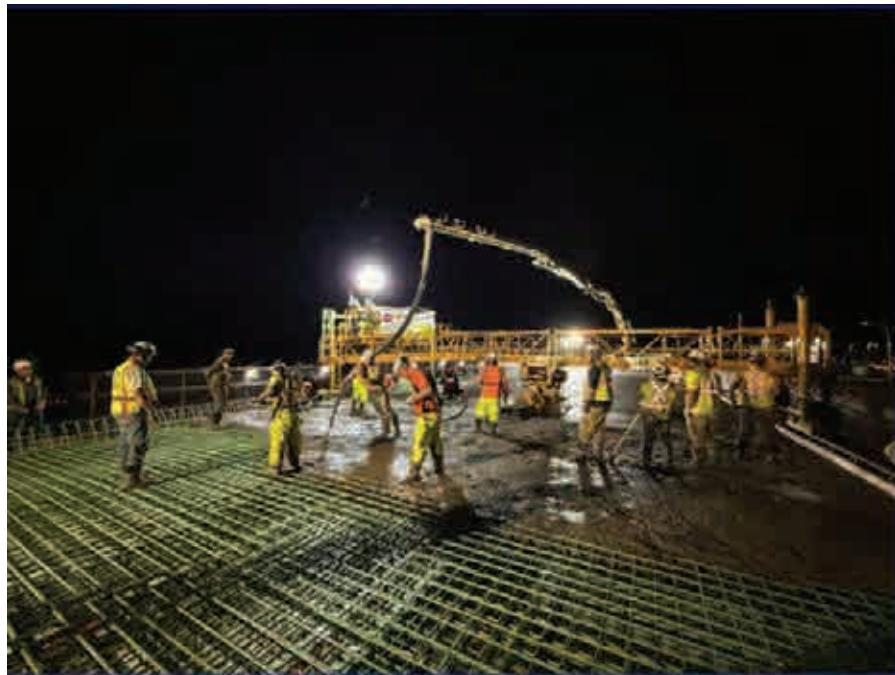
Pier 3 Stepped Footing Piles

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Deck Concrete Placement (N-12-066)



Weekend One Demolition (Bridge N-12-067)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

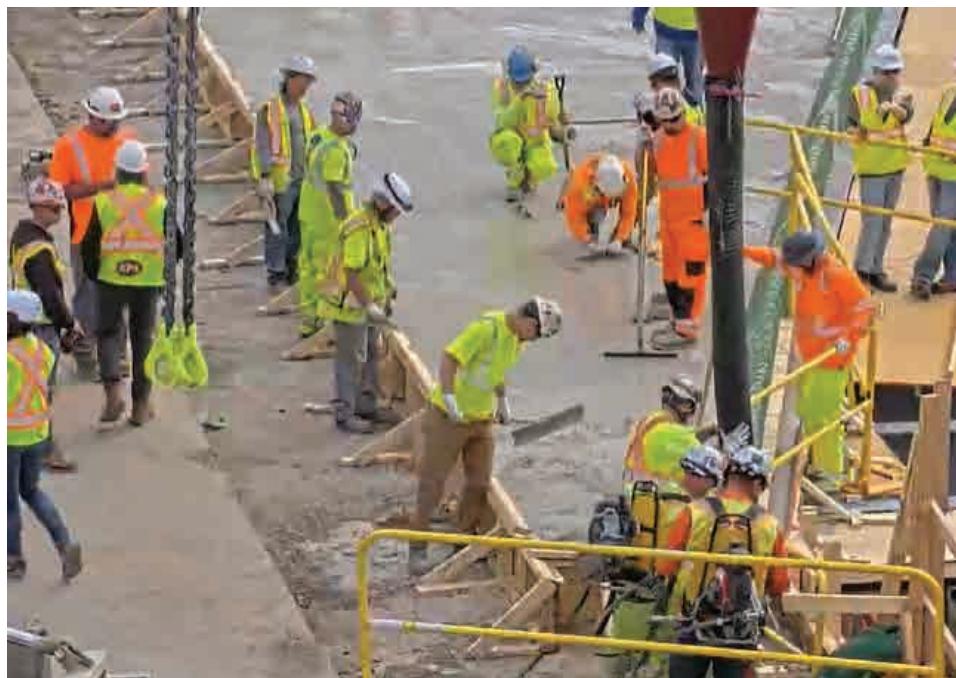
Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Weekend One Installation of Precast Bridge Units (N-12-067)



Weekend One Closure Pours (Bridge N-12-067)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Weekend Two Demolition (Bridge N-12-067)



Weekend Two Precast Bridge Units (Bridge N-12-067)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Weekend Two Closure Pours (Bridge N-12-067)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

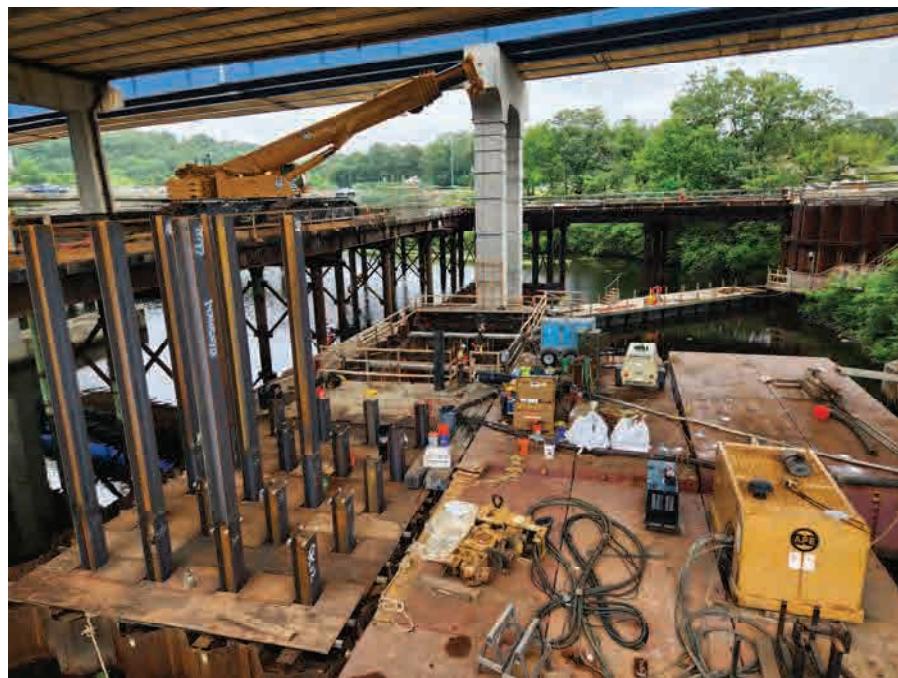
Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Bridge Deck Paved Surface (Bridge N-12-066)



Pier Two Footing Piles (Bridge N-12-066)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Wall 20 with Stain Applied



Traffic Shifted to Stage 1 Eastbound Roadway (Bridge N-12-066)

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Demolition of Existing Eastbound Bridge N-12-066 Superstructure



South Abutment Bridge W-29-057

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



North Abutment Bridge W-29-057



South Abutment Bridge W-29-058

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Girders Installed at Bridge W-29-057



W-29-057 Deck Concrete Placement

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Bridge W-29-055 Abutment and Pier Construction



Bridge W-29-058 Abutment and Pier Construction

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Girders Installed at Bridge W-29-055



Bridge N-12-066 Pier 2

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259



Bridge N-12-066 Pier 3

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

Keville Enterprises performed an independent review of the Construction Time Determination in May of 2022 to provide concurrence with the anticipated project duration.

Value Engineering

A Value Engineering study was not performed however the Design-Build procurement included a confidential Alternative Technical Concept process.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues.

Extra Work Orders

As of November 30, 2025, there are four approved extra work orders.

1. MSE wall rebar changed to epoxy coated.
 - a. Disposition: approved
 - b. Cost: \$129,366.31
2. Bridge N-12-065 Complete Replacement Design.
 - a. Disposition: approved
 - b. Cost: \$2,194,796.05
3. Inspector Vehicle Contract Purchase.
 - a. Disposition: approved
 - b. Cost: \$36,876.40

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

4. Inspector Vehicle Contract Purchase.

- a. Disposition: approved
- b. Cost: \$38,361.40

As of November 30, 2025, there is one pending extra work order:

5. Bridge N-12-065 Full Bridge Replacement.

- a. Disposition: pending
- b. Cost: \$72,035,844

As of November 30, 2025, there is one probable extra work orders:

6. Bridge N-12-064 Deck Replacement.

- c. Disposition: probable
- d. Cost: \$29,000,000

Project Name: Newton Weston I-90 over I-95 Charles River, Charles Street and MBTA/CSX

Owner's Representative Contract Number: 120632

Project Number: 606783

Construction Contract Number: 120259

OWNER'S REPRESENTATIVE OATH

I Keith V. Lincoln, MA P.E. No. 42567, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120632, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



Keith V. Lincoln, P.E.

Date: December 16, 2025



Commonwealth of Massachusetts

Maureen Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Bridge Bundle, Replacement of Bridges at I-495/Route 28 Interchange and Over the MBTA Railroad in the Town and City of Andover-Lawrence

Owner's Representative Contract Number: 120636

Project Number: 606522

Construction Contract Number: 121394



Owner's Representative's 2025 Annual Report

Owner's Representative Contract No. 120636—Millennium Engineering, Inc.

Project No. 606522: Bridge Bundle, Replacement of Bridges at I-495/Route 28

Interchange and Over the MBTA Railroad in the Town and City of Andover-Lawrence

Preliminary Designer: Chappell Engineering Associates, LLC

Design-Builder: The Middlesex Corporation

Prepared by: Millennium Engineering, Inc

December 16, 2025

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I. EXECUTIVE SUMMARY

The Andover-Lawrence Bridge Bundle, Replacements at I-495 over Route 28 and MBTA is a design build project to design and construct six bridge replacements located in the town of Andover. Bridges to be replaced include I-495 NB and SB over Route 28 NB, Route 28 SB and the MBTA Commuter Rail just to the north of the Route 28 interchange. The project also includes geometric and safety improvements on I-495 between Corbett Street in Andover to the I-495/Route 114 interchange in Lawrence.

As a regionally and nationally significant roadway, I-495 sees high daily vehicle volumes. On an average day, about 113,000 and 30,000 vehicles use I-495 and Route 28 respectively. I-495 also saw 206 reported crashes between 2012 and 2017, including 2 fatalities. Many of these crashes occurred while merging and diverging from I-495 at the Route 28 and Route 114 interchanges.

The project limits extend along Interstate 495 from the Corbett Street overpass in Andover to approximately 400 feet west of the I-495 Bridge over Route 114 in Lawrence. Within these limits are milling and overlay, minor widening, and safety improvements to I-495, the I-495 ramps to and from Route 28 NB and SB and to Route 28 NB and SB in the area under the bridges. The approximate project length on I-495 is 1.06 miles and on Route 28 is .16 miles. The length of the project on I-495 is primarily determined by the need for crossovers to manage traffic during construction.

The horizontal and vertical alignment of I-495 is generally retained, except this project includes operational and safety improvements. Shoulders within the Route 28 interchange areas will be widened to help provide recovery area and emergency stopping/breakdown space. A full-length acceleration and deceleration lane is proposed between the Route 28 and Route 114 interchanges along both I-495 NB and SB to improve merge/diverge operations.

The I-495 NB and SB typical cross section within the Project consists of three 12-foot travel lanes with 12-foot wide minimum inside shoulders and 12-foot minimum outside shoulders. There is a 28-foot-wide grass median. Within the Route 28 NB and SB interchange, the I-495 NB and SB typical cross section consists of three 12-foot travel lanes, a 12-foot acceleration/deceleration lane, and a 12-foot minimum overbuild area in the median and a minimum 10 foot inside and outside shoulder. The “overbuild” area is a result of the construction of the bridges and crossovers in the median area during Phase 1 construction. A concrete median barrier will separate the NB and SB barrels. A full-length acceleration and deceleration lane will be provided between the Route 28 and the Route 114 interchanges along I-495 NB and SB.

A locus map of the study area is shown in Figure 1. This project is being constructed by the

Massachusetts Department of Transportation (MassDOT) - Highway Division with Chappell Engineering Associates, LLC as the Preliminary Designer of Record. The project was procured as a Design-Build Contract with Technical Proposals reviewed in May-June 2023 and Price Proposals opened June 28, 2023, with the Best Value being the Design-Build Team of Middlesex Corporation of Littleton, MA as the Prime Contractor and WSP as Design-Build Designer of Record. Notice to Proceed was issued to Middlesex on August 22, 2023.

While the project is currently on schedule there have been several Non-Conformance issues that have caused Middlesex to work extra shifts to keep the project moving forward. As a result of these issues the completion of stage 1 won't occur until next spring. However, Middlesex is confident that in 2026 they will be able to complete both stage 1 and stage 2 work to keep the project on schedule. MassDOT is working closely with Middlesex to improve their Quality Control program in an effort to eliminate any future Non-Conformances.

Figure 1 - Project Location



II. ANNUAL REPORT

Contract Scope of Work

Proposed improvements include:

- The full replacement of the six bridges carrying I-495 NB and SB traffic over Route 28 NB and SB and the MBTA. The new structures will include full substructure and superstructure replacements consistent with the cross section of the approach roadways. The structures will meet current statutory loading requirements as well as seismic requirements and all applicable bridge design criteria.
- Reconstruction of I-495 and Route 28. The roadway reconstruction will be a combination of mill and overlay of the existing roadway with full depth construction to achieve super elevation.
- Local widening of I-495 to provide wider shoulders and improved acceleration and deceleration lanes between the Route 28 and Route 114 interchanges.
- Minor widening on Route 28 to facilitate bicycle lanes and provide ADA compliant ramps and sidewalks.
- Highway lighting improvements to illuminate the ramps and auxiliary lanes.
- Utility coordination, protection, and relocation including coordination with utilities, MBTA (Keolis), CSX and Amtrak for the replacement of bridges over active tracks, coordination with National Grid for the relocation by National Grid from overhead to underground of power lines along the MBTA right-of-way, coordination and relocation of MBTA Positive Train Control (PTC) cable and conduit, and protection in place of MassDOT ITS infrastructure and relocation of ITS camera and support.
- Drainage upgrades include improvements and modifications to drainage systems for temporary and permanent conditions, construction of stormwater control measures for water quality improvements, and installation, maintenance, and removal of all erosion control protections required for work activities.
- The structures will meet current statutory loading requirements as well as seismic requirements and all applicable bridge design criteria.

Major Progress As Of November 30, 2025

During 2025, The Middlesex Corp made numerous design submittals in support of the proposed construction activities, the most significant are: 100% Highway Design; Construction Staging Plan; Early Release for Construction Northbound Crossover Temporary Traffic Control Plan; Temporary Lighting; Monthly Schedule Updates #14-25 & Structural Designs of all bridges.

In addition, the following construction work has taken place:

- Stage 1 traffic shifts on I-495 NB & SB
- Stage 1 traffic shift for Route 28 NB & SB
- Stage 1 demolition of all 3 bridges
- Installation of earth support systems for excavation for the abutments
- Driving piles for stage 1 bridge abutments.
- Construction of MSE walls for all stage 1 abutments
- Forming, setting rebar, and placing concrete for all stage 1 abutments
- Setting Next F beams for all stage 1 bridge decks
- Forming & setting, rebar for all stage 1 bridge decks
- Placing concrete for bridge deck at bridge 37
- Drainage work in 495 median to support traffic crossovers.
- Paving I -495 for crossovers
-

Budget

- Office Estimate: \$126,017,432
- Price Proposal: \$ 98,080,330
- Encumbered Amount: \$ 116,715,846
- Total Amount Expended through November 30, 2025: \$44,273,308
- Current Estimate at Completion: \$ 107,609,673

- The table below contains a summary of project financials through November 30, 2025:

Project Budget / Financials			
	<u>Contract</u>	<u>MassDOT</u>	<u>Municipal</u>
Bid	\$ 98,080,330	\$ 98,080,330	\$ 0
Allowances		\$ 8,818,558	\$ 0
	\$ 8,818,558		
Original Contract Value	\$ 106,898,888	\$ 106,898,888	\$ 0
Original Contingency (FIN681)		\$ 9,816,958	
Original Encumbrance		\$ 116,715,846	
Encumbrance Modifications		\$ 0	
Current Encumbrance		\$ 116,715,846	
Current Overruns and Underruns	\$ 192,500	\$ 192,500	\$ 0
Approved Contract Modifications	\$ 366,853	\$ 366,853	\$ 0
Current Contract Value	\$ 107,458,241	\$ 107,458,241	\$ 0
Pending Contract Modifications		\$ 151,432	\$ 0
	\$ 151,432		
Estimated Contract Total	\$ 107,609,673	\$ 107,609,673	\$ 0
Probable Contract Modifications		\$ 0	
Forecast Cost-at-Completion	\$ 107,609,673	\$ 107,609,673	\$ 0
Total CQE (Invoiced) to Date	\$ 44,273,308	\$ 44,273,308	\$ 0
Funds Remaining		\$ 72,442,538	
Projected Funds Remaining		\$ 9,106,173	
Expected Progress (baseline)	0.00%		
% Complete	41.14%		

Extra Work Orders

EWO #	Item Description	Status	Cost
1.	Material Change to High Performance Concrete for Barrier	Approved	\$52,422.75
2.	Zone Guard Barrier Rental	Approved	\$290,818.09
3.	Drainage Revisions along I-495 for Binney Street	Approved	\$88,688.62
4.	Material Change to 5000 psi Concrete for Shotcrete	Approved	\$12,557.37
5.	Washout at Bridge #37	Approved	\$35,632.88
6.	Obstruction Near Electric Manhole #1-MBTA	Approved	\$43,252.47
7.	Shoring Towers at Bridge 36 Girders 1 & 2	Approved	\$63,537.20
8.	MBTA Track Monitoring	Approved	\$60,432.60
9.	Remove Obstruction along the MBTA Right-of-Way	Approved	\$12,779.10

Schedule / Project Milestones

- Contract Award: August 3, 2023
- Notice to Proceed: August 22, 2023
- Substantial Completion November 4, 2027
- Final Completion: December 27, 2027

Areas of Concern

While the project is currently on schedule there have been several Non-Conformance issues that have caused Middlesex to work extra shifts to keep the project moving forward. As a result of these issues the completion of stage 1 won't occur until next spring. However, Middlesex is confident that in 2026 they will be able to complete both stage 1 and stage 2 work to keep the project on schedule. MassDOT is working closely with Middlesex to improve their Quality Control program in an effort to eliminate any future Non-Conformances and to ensure that the project schedule is met.

Progress Photographs



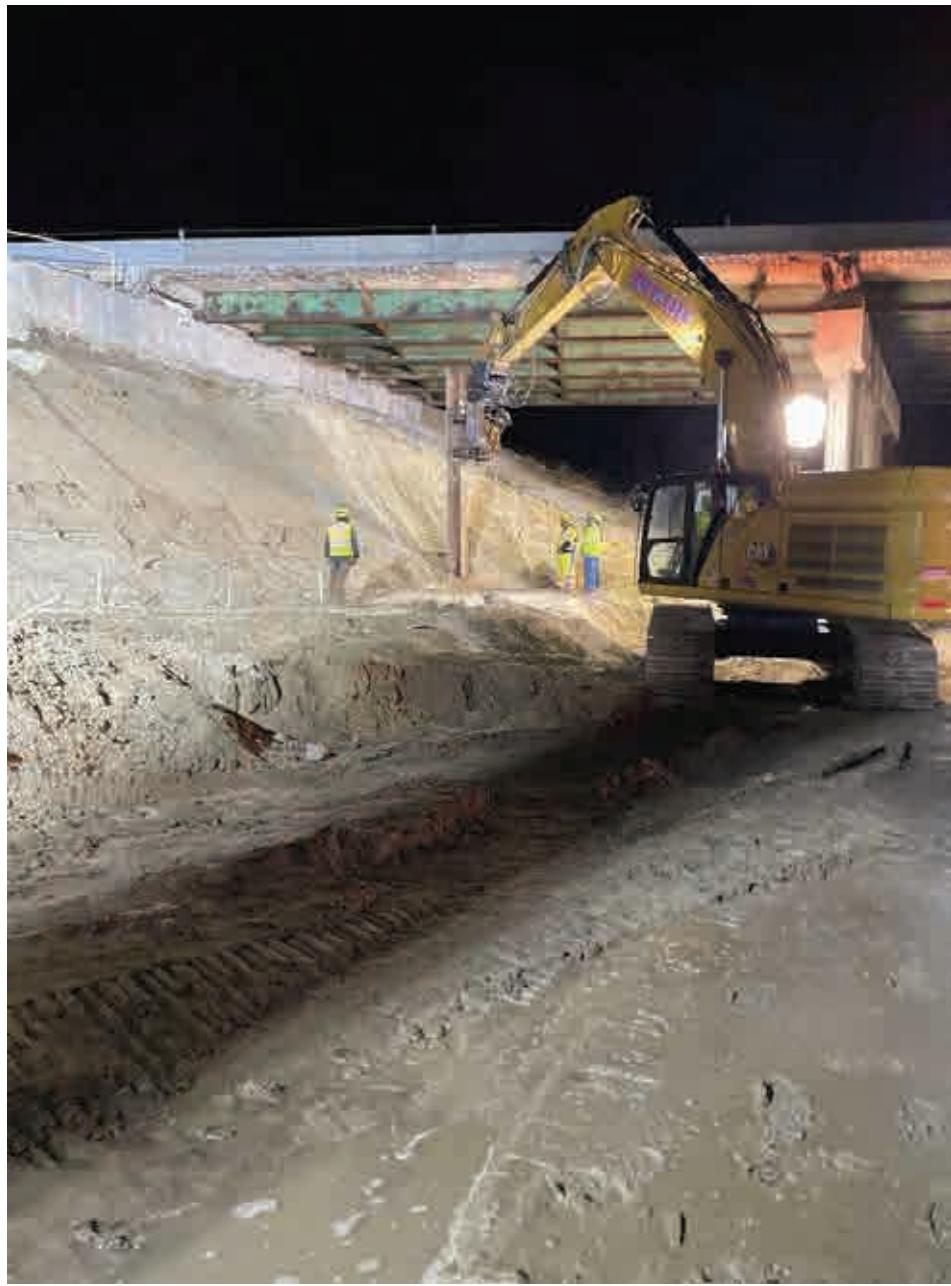
Bridge Demolition at Route 28

Project Name: Bridge Bundle I-495 over Route 28 & MBTA Railroad Andover-Lawrence
Owner's Representative Contract Number: 120636
Project Number: 606522
Construction Contract Number: 121394



Soil Nail Wall

Project Name: Bridge Bundle I-495 over Route 28 & MBTA Railroad Andover-Lawrence
Owner's Representative Contract Number: 120636
Project Number: 606522
Construction Contract Number: 121394



Pile Installation for Support of Excavation

Project Name: Bridge Bundle I-495 over Route 28 & MBTA Railroad Andover-Lawrence
Owner's Representative Contract Number: 120636
Project Number: 606522
Construction Contract Number: 121394



Pile Installation for Integral Abutment

Project Name: Bridge Bundle I-495 over Route 28 & MBTA Railroad Andover-Lawrence
Owner's Representative Contract Number: 120636
Project Number: 606522
Construction Contract Number: 121394



ME Wall Installation at Abutment



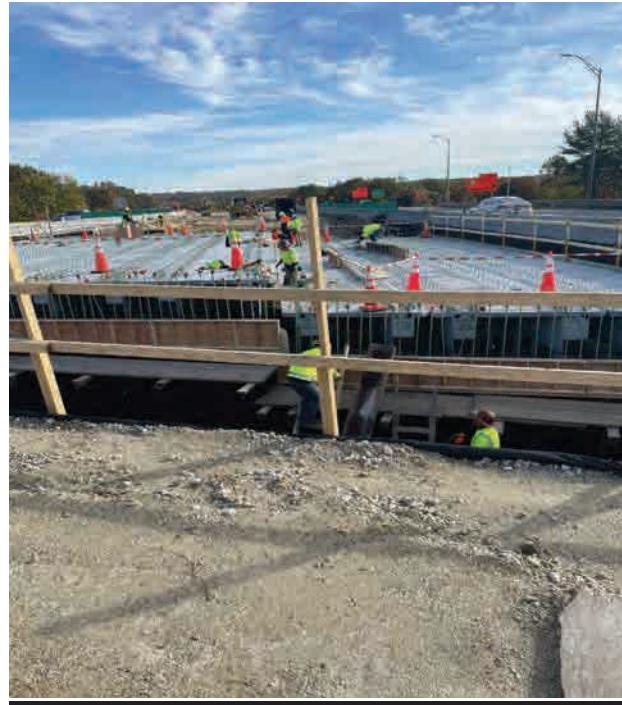
Forming Abutment

Project Name: Bridge Bundle I-495 over Route 28 & MBTA Railroad Andover-Lawrence
Owner's Representative Contract Number: 120636
Project Number: 606522
Construction Contract Number: 121394



Paving for Crossover

Project Name: Bridge Bundle I-495 over Route 28 & MBTA Railroad Andover-Lawrence
Owner's Representative Contract Number: 120636
Project Number: 606522
Construction Contract Number: 121394



Next F Beams



Placing Concrete for Bridge Deck

- **CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST**

Peer Review

A peer review was not performed prior to Design Build Procurement.

Value Engineering

A Value Engineering (VE) Study of the project design concept was not performed.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues.

OWNER'S REPRESENTATIVE OATH

I Eric W. Botterman, MA P.E. No. 41099 hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120636, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and **penalties of perjury**.



Eric W. Botterman, P.E.

Date: December 16, 2025

Owners Representative
Report, October 2025

General Project Data

Project: **613005** DISTRICT 5- OFF-SYSTEM BRIDGE BUNDLE, D-14-008, L-01-011, L-01-01...

Project Type: **Bridge Deck Replacement** Fed-Aid: Yes Location: **DUXBURY - LAKEVILLE - MAN...**

District: **District 5** Primary Design Consultant: **CHA Consulting, Inc.**

Engineer-of-Record:

Contract: **121839** Contractor: **SKANSKA USA CIVIL NORTHEA...** OR Contract Number: **120634**

Safety

A Health and Safety Plan was submitted (EHASP - Submitted Rev02 on 3-11-24). Its objective is to identify the EHS impacts of the company's activities, products and services and to develop operational controls, indicators, measurable objectives and targets as defined in the EHS Manual. This assessment will take into account our operations, subcontractor operations, visitors and vendor activities. EHS impacts will be identified before the start of work and updated as needed.

DB-Team conducting weekly safety meetings with crews. Wednesday's @ 10am for Weekly Toolbox talks at each Bridge Location and Exit 22 for Skanska crews.

Quality Control

The Quality Management Plan (QMP) has been developed to ensure that real time information is available to monitor and control the quality of all work. The QMP formalizes the lines of responsibility and the QC organizational structure, document management and Design and Construction QC procedures. QMP rev03 to be submitted to MassDOT.

CSI QSM Rev01 R&R

Construction Quality Control Plans being submitted and process in place for submittal, review and approval with no indications of delay.

Design-Builder to open NCR-03 Jordan Rd. Auciello fabrication of the S3-MTL4 rail, Bottom rail HSS 5x5x1/4 has 18 mis-drilled holes due to incorrect programming of the fabricator's CNC Machine. Status: STEP 9 MassDOT verification of Corrective Action.

River St.: Schedule BM #9 SP #2 Mag Particle test and drill hole to arrest the crack. MT testing was performed 10/16, waiting on report to share with MassDOT. Tip of the crack was right below the web on the bottom flange, so it was advised to mark the crack and monitor if it propagates. If it propagates further on the bottom flange or the web, drill arrest hole. Fenagh Engineering Mag Particle Test 10/29 identified crack propagating.

Community Outreach

Community Outreach will be handled by Regina Villa Associates (RVA). RVA to send updates to inform public of upcoming activities.

DB Team to inform the Town of Plymouth of the next Full PPH closure for Girder #9 install, target November 16, Sunday

Recent Major Progress

- Weekly Internal check-ins held among the MassDOT Team
- Design Build Team Coordination meetings 10/8, 10/22

FDC-013: Field Design change with Utility modifications from Eversource and Verizon. Submitted to MassDOT 10/1/2025.

Pedestrian Crosswalk at Warren Ave. MassDOT accepted Design proposal 10/16. Benesch working on Design and AAB.

East St. Utility work and Concrete Pours (Sidewalk, Wingwall extensions, Header Beam, Curb).

River St. SIP's and Girder #9 replacement.

George St.: Install curb, Sidewalk, Substructure and Stairway repairs and HMA Paving.

Jordan Rd. Bearing replacements and Steel repairs.

Pickens St. Demolition

Charge Pond Rd. Sand Blast and Prime application Span #2 and abutments.

Upcoming Events/Milestones

Project continues in the design/submittal phase.

East Street (Duxbury) Conduit Core Drill Backwall, Strip Abutment Header, Backfill Abutments, Grout Screed Rail Holes, Fence Bolt Installation, Curb & Sidewalk Rebar Inst, Inst Deck Scuppers, Form Sidewalk, Strip Sidewalk Sect 1, Form Curb Sect 1, Strip Curb Sect 1, Form Curb Sect 2,m

River St. (Plymouth) Install Lagging over Road (Span 2), Install New Beam (Span 2 Girder #3) - DCN 001, Install Diaphragms & Bolt Up New Girder, Install SIPs

George St. (Mansfield) Installation of Guardrail, Paving

Jordan Rd. (Plymouth) Gemstone - Top flange Girders + Diaphragm, Demolition (Overhang Capacity Verification approval required)

Pickens St. (Lakeville) Install Anchor Bolts for Highway Guard, Install Temp Attenuator / Traffic Control, Demolition

Charge pond Road (Wareham) -Deleading - Median

Discussion/Concerns of Performance

None at this time.

Potential Cost Recovery Issues

None at this time.

Schedule Recovery / Mitigation (As of 10/15/2025)					
Event	Original Contract	Current Contract	Contractor's Forecast	Variance	District/OR Forecast
Milestone No. 2 – Full Beneficial Use/Substantial Completion	10/05/2026	10/05/2026	12/30/2026	(86 Days)	10/05/2026
Milestone No. 1 – Design-Builder Field Completion	11/30/2026	11/30/2026	02/25/2027	(87 Days)	11/30/2026

Forecast Variance

None at this time.

Schedule Notes

A detailed construction schedule has been submitted and will be maintained using Primavera Project Planner P6. The activities will be segregated into a project-specific WBS to facilitate tracking the progress of both design, procurement, and construction activities. Submittal of critical work activities, milestones, material fabrication, and delivery will also be shown to enable the management team to continually assess the progress of the project. The project schedule will be updated on a monthly basis and reviewed by the Management Team at the Project Meetings. The Project Scheduler will assist in identifying long lead-time items early in the process. As part of our QC management plan, plant visits will occur periodically to inspect the quality and monitor the production of major permanent materials.

90-day Look Ahead submitted bi-weekly

3 Week Look Ahead being submitted weekly on Thursday's

Schedule Update No. 24 reviewed and returned by MassDOT on 10/23 days negative float).

Schedule Update No. 25 submitted to MassDOT 10/23.

Final TEA - 04: Girder #3 Span #2 replacement at River St. sent to MassDOT 10/30. Request for Time extension w/associated cost.

Project Budget / Financials			
	Contract	MassDOT	Municipal
Bid	\$ 36,900,000	\$ 36,900,000	\$ 0
Allowances	\$ 1,736,894	\$ 1,736,894	\$ 0
Original Contract Value	\$ 38,636,894	\$ 38,636,894	\$ 0
Original Contingency (FIN681)		\$ 3,690,000	
Original Encumbrance		\$ 42,326,894	
Encumbrance Modifications		\$ 0	
Current Encumbrance		\$ 42,326,894	
Current Overruns and Underruns	\$ 919,356	\$ 919,356	\$ 0
Approved Contract Modifications	\$ 1,064,709	\$ 396,411	\$ 668,298
Current Contract Value	\$ 40,620,960	\$ 39,952,662	\$ 668,298
Pending Contract Modifications	\$ 404,196	\$ 404,196	\$ 0
Estimated Contract Total	\$ 41,025,156	\$ 40,356,858	\$ 668,298
Probable Contract Modifications		\$ 0	
Forecast Cost-at-Completion	\$ 41,025,156	\$ 40,356,858	\$ 668,298
Total CQE (Invoiced) to Date	\$ 17,438,898	\$ 16,770,600	\$ 668,298
Funds Remaining		\$ 24,887,996	
Projected Funds Remaining		\$ 1,970,036	
Expected Progress (baseline)	0.00%		
% Complete	42.51%		

Cost Notes

Pay Estimates cutoff date for Bridge Bundle Project moved to end of each month (29-30th).

Pay Req #25 month of October: draft ready to submit once pay req 24 is returned.

Cost Control/Mitigation Notes

PCO 001, Water line replacement for East Street, updated pricing sent on 5-16-24. DB Contractor awaiting PCO approval. Long lead materials ordered.

PCO 002, Geotechnical Memo and Borings at retaining wall locations, proposal sent 5/3/24. Received Official approval letter 9/19/24

PCO003: Design-Builder evaluating impacts of RFI#008 and to submit to MassDOT (extra cost/credit). Benesch responded to MassDOT comments 4/7/2025. Meeting to be set up to further discuss the comments. Design-Builder working on the Construction net cost for moving from 2-Staged construction to a single stage. Design-Builder submitted Cost proposal to MassDOT 6/5/2025. Skanska sent proposal with cover sheet 8/11/25. Executed EWO-008 MassDOT received by Design-Builder on 9/12/2025

PCO004: Steel Diaphragm replacement: Engineering costs. Clarification on Steel repairs. Skanska submitted proposal to MassDOT 9/19/2024.

PCO005: East St. Water line bypass estimate. Town of Duxbury have proposed the use of different water pipe supports. Approved by MassDOT. Received executed document from MassDOT 5/8/25.

PCO006: collision damage (not previously known) at Jordan Rd. Proposal not submitted officially. Rough order of magnitude with options to repair girder and a new steel girder submitted to MassDOT 3/27/2025. Executed document received 6/12/25

PCO007: collision damage (not previously known) at Pickens St. MassDOT requesting accident report. Draft Proposal submitted to MassDOT 5/9/2025

PCO-009: Pedestrian Crosswalk at Warren Ave. (Plymouth). Pricing submitted. Supplemental EWO 3.1 to

be studied by Design-Builder.

PCO-011: Crack development at River St. after Heat Straightening. Beam Replacement Option was again discussed. DB-Team provided cost estimate for Spanner Beam Option against Beam Replacement Option through RFI #023. MassDOT provided direction to start design of Girder 4/9/2025. Saugus to revise Equipment rates, Truck with tools, and adjust Saugus narrative not including estimates from other jobs.

PCO-012: Bearing seat repair Beam #4 South Abutment George St. Tracking T&M. MassDOT suggests this to be covered through Unit price for concrete repairs (Pay item 905). Design Builder monitoring cost and quantities. Total Quantity: 0.02 CY @ \$130 per the \$6,500/CY as item 905 in the Contract. Not submitted to MassDOT. Labor costs approx. \$3,500

PCO-014: Global Stability Analysis added to the Geotechnical design of the retaining walls. Executed EWO-002.1 MassDOT received 2/14/2025

PCO-015: BTC Survey discrepancies at Pickens St. - Profile adjustments during design. Notice of Change sent to MassDOT 2/19/2025 - Denied.

PCO-017: Contech solution's proposal for clear poly SIP shared with MassDOT 2/18/2025. This will be submitted as a separate PCO adding net cost (credit for not using removable forms, added cost to install clear SIP on utility bay). Draft Cost Submitted (need to update Benesch and Saugus). Comments from MassDOT responded 4/23/25. Submitted to MassDOT 5/13/25. Saugus Comments, Equipment watch rates. Submitted to MassDOT new revision 6/24.

PCO-018: Asphalt pavement limit changes at George St. Comments from MassDOT 8/13 responded by Skanska. New Cost and cover letter sent to MassDOT 8/25. Executed EWO-009 MassDOT received by Design-Builder on 9/12/2025.

PCO-019: Utility modifications at East St. In progress.

PCO-020: River St. Beam #9 Span #2 replacement. Draft cost proposal submitted to MassDOT 8/21. Duration: 8 - 10 Weeks total from receiving Direction to Proceed to have girder delivered to the Job Site. DB putting together final estimate once draft design has been completed.

PCO-021: Amtrak/MBTA Fence at George St. Meeting 9/12 @ 9:00 am George St. DeLucca to submit pricing for this operation (Cost of temporary solution, GPR Concrete Scanning, DeLucca fence F&I)

PCO-022: Jordan Rd. findings after demolition. No concrete Slab between NB and SB Structures over Route 3 and no approach slabs identified at the West abutment of the NB Jordan Rd. Structure and the East abutment of the SB Structure.

PCO-023: Cracks identified by MT/UT Testing at Pickens St. would be considered Supplemental EWO to PCO-007.

Owner's Representative Summary

Miscellaneous Notes

Project Scope

The purpose of this project is to preserve eight non-NHS bridges throughout District Five. All of the bridges are on local roads and are exempt from MassDOT's Bicycle Controlling Criteria. However, all of the bridges will only have one sidewalk, and therefore require an exception for number of pedestrian facilities.

Environmental, Cultural and Community Updates

A Categorical Exclusion (CE) has been obtained for the Project and was provided to the selected Design-Builder.

- The Design-Builder will be responsible for minimizing impacts to environmental resource areas.
- The Design-Builder will be responsible for adhering to all General and Special Conditions contained within the environmental permits.
- If the Design-Builder proposes any changes to the design or construction impacting resource areas, the Design-Builder will be responsible for obtaining necessary permit amendments and regulatory approvals.

Northern Long-eared Bat (NLAA) assessment: Concurrence verification letter received from the Fish and Wildlife Service dated 10/2/2024, indicating that the project is consistent with a not likely or adversely affect determination of the Northern Long Eared Bat or the endangered Indiana bat. Timing of Bridge Work mentioned in the letter is Fall 2024 - Summer 2025.

Meeting 9/12 @ 9:00 am George St. DeLucca to submit pricing for this operation (Cost of temporary solution, GPR Concrete Scanning, DeLucca fence F&I).

Utilities

No Update

Amtrak

No Update

Major Concerns

I, **Paul Tyrell**, hereby certify that my sole responsibility as Owner's Representative, under OR contract No. **120634** is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this monthly report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

Signature: Paul Tyrell

11/17/2025

Date



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative Contract No. 120633 – Greenman-Pedersen, Inc.

Project No. 605959 – Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

BOSTON-CHELSEA, MASSACHUSETTS

Preliminary Designer: TranSystems, Inc.

Contractor: Liberty Maintenance, Inc. – SPS Joint Venture

Prepared by: Greenman-Pedersen, Inc.

December 15, 2025

Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

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Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

I. EXECUTIVE SUMMARY

This is a bridge preservation contact for the Chelsea Approach portion of the Tobin Bridge. This portion of the Tobin Bridge is referred to as Bridge No. B-16-017=C-09-006 (4X0, 4WY) US 1 NB & US 1 SB over Chelsea Local Streets.

The project includes a full clean and painting of the structural steel, substructure concrete patching, miscellaneous superstructure steel repair, safety walk repair, and associated traffic control. A location map of the project area is shown in Figure 1. This project is being constructed by the Massachusetts Department of Transportation (MassDOT) - Highway Division with TranSystems, Inc. as the Designer of Record.

The project was procured as a traditional Design-Bid-Build Contract with Bids opened August 29, 2023. The low bid was submitted by a Joint Venture of Liberty Maintenance, Inc. and SPS New England, Inc., with a Notice to Proceed issued on October 25, 2023.

The project is approximately 73% spent, which is consistent with the cost loaded project schedule.

The project is on-schedule with a current completion date of November 12, 2027.

The primary purpose of this project is to preserve the existing structure and maintain a State of Good Repair by removing existing lead paint, installing a new paint system, and repairing deteriorated steel and concrete bridge elements.

The limits of the project are bordered by previous MassDOT projects. This projects northern limit abuts the completed Chelsea Viaduct project (605287), and the southern project limits abuts the previous Tobin Bridge Main Span painting project in 2011 (605947).

The following images show the overall project location and the specific project limits with references to specific support bents, ramps, and roadways.

Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602



Figure 1 - Project Location

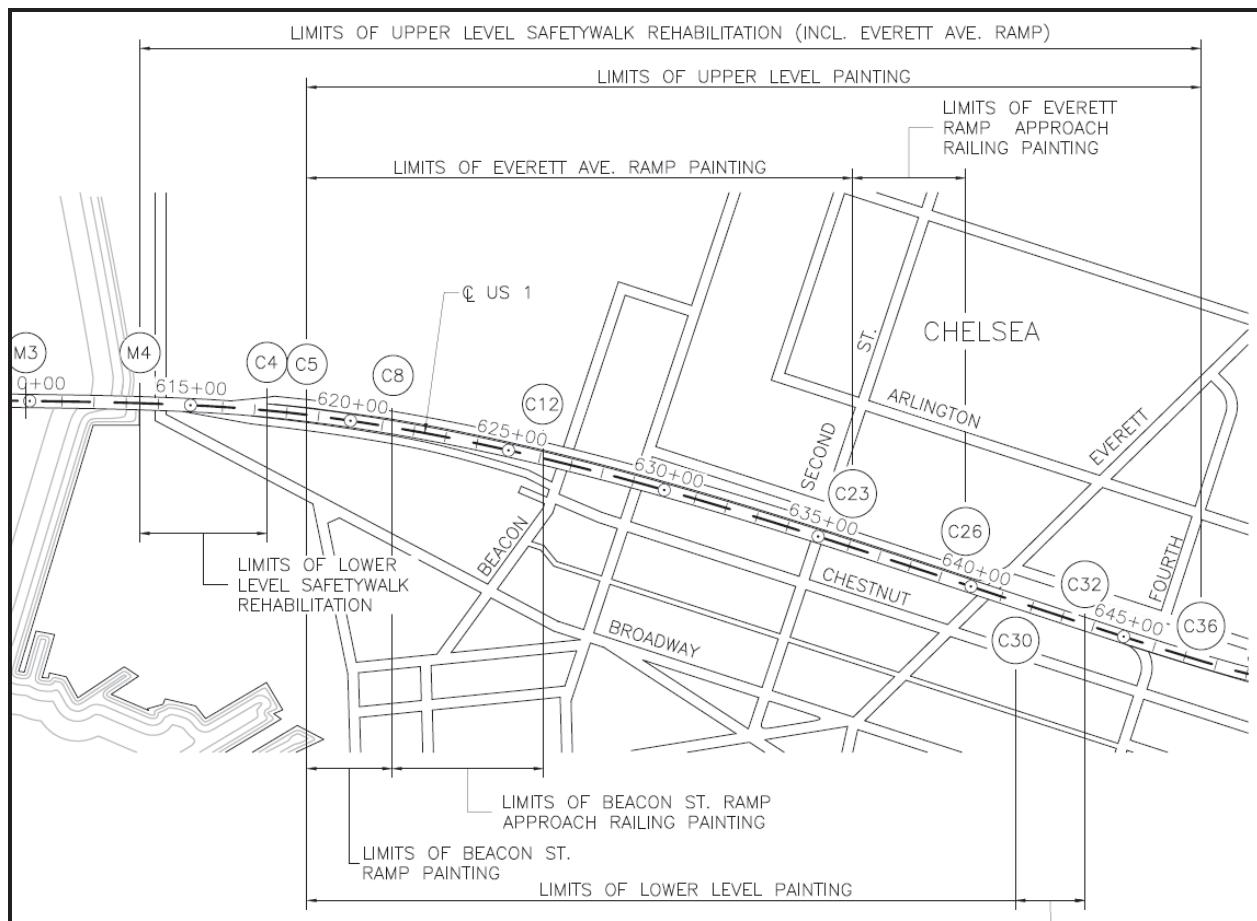


Figure 2 – Specific Project Limits

II. ANNUAL REPORT

Contract Scope of Work

The work under this contract consists of the preservation of Bridge No. B-16-017=C-09-006 (4X0,4WY), US 1 NB & US 1 SB over Chelsea Local Streets. The majority of the preservation work covers approximately 3000 linear feet of northbound roadway (lower level) and 3350 linear feet of southbound roadway (upper level) of US 1, along with approximately 950 linear feet of work at the Beacon Street Ramp and 2,300 linear feet of work at the Everett Avenue Ramp. The work to be performed consists of, but is not limited to, the following:

- Erect temporary protective shielding/work platforms/containment as necessary to contain the flaking paint chips and to perform the work.
- Place temporary barriers and other necessary temporary traffic controls along the lower level (US 1 NB), Beacon Street Ramp, Everett Street Ramp, and upper level (US 1 SB) in accordance with the Temporary Traffic Control Plan.
- Full clean of existing steel on lower level mainline (US 1 NB), Beacon Street Ramp, Everett Street Ramp, and upper level mainline (US 1 SB). Perform repairs to existing steel superstructure elements, and safety walk elements at lower level mainline, Beacon Street Ramp, Everett Street Ramp, and upper level mainline. Upon completion of steel repairs, full paint of the existing and new steel. Perform repairs to substructure elements.
- Complete all other incidental work necessary to complete the proposed preservation and remove the temporary protective shielding/work platforms and any remaining temporary traffic controls associated with the Temporary Traffic Control Plan.

Major Progress As Of November 30, 2025

The following is a detailing of events which have transpired related to 2025 construction activity.

- From January to April the colder weather does not allow for painting, thus the project was less active and focused on localized structural steel repairs.
- Blast, clean, and paint operations started in late April and focused on the east side of the Mainline bridge, as well as mobilizing for the Beacon Street off-ramp
- Through the summer, project wide cleaning, painting, as well as steel and concrete repair work advanced. The contractor also started preparing for painting of the Route 1 over Webster Avenue bridge that was added as extra work to the project.
- Roadway barrier was replaced on Route 1 NB left shoulder at the northern limits of the project, as part of an extra work order.
- In early fall, the Webster Avenue bridge painting was completed using accelerated weekend work windows. The Beacon Street off ramp painting was substantially completed and mobilization began on the Everett Street on ramp.
- Through October and November, the project finished painting on many of the mainline support steel bents, cleaned and primed the Everett Street on-ramp, and demobilized from the Webster Avenue bridge.
- In late November, work began on electrical switch gear replacements and luminaries along Beacon Street, as part of extra work orders added to the project.
- As of November 30, many of the containment tarps have been removed for the winter season. Miscellaneous steel repairs are continuing through the colder weather.
- 2026 will focus on finishing the Everett Street on-ramp, as well as structural steel repairs and concrete repairs though the entire project area.

Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

Budget

The following is a summary of the Budget as of November 30, 2025. This project has no Municipal Encumbrance and no Federal Participation.

• Office Estimate:	\$122,568,952
• Low Bid:	\$109,145,207
• Current Encumbered Amount:	\$132,353,088
• Total Amount Expended through November 30, 2025:	\$89,672,735
• Current Estimate at Completion:	\$122,756,488

Schedule / Project Milestones

- Notice to Proceed: October 25, 2023
- Substantial Completion (Full Beneficial Use): September 20, 2027
- Final Completion: November 12, 2027

Areas of Concern

The following represents the primary areas of concern identified in design and at this stage in construction:

- The local community continues to express concerns about paint chips, noise, dust, and paint smell from the project. Weekly the contractor and MassDOT walk the project area and pick up paint chips that are found. Substantial amounts of steel have already been cleaned and painted, thus the expected amount of paint chips, and overall project dust, should continue to diminish.
- The project involves rehabilitation of steel and concrete bridge elements that are difficult to quantify during design development. As rehabilitation work progresses, additional unseen areas of deterioration may be identified that should be addressed in this contract.

Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

Beacon Street On-Ramp

Photographs showing steel in preparation for blast clean operations, cleaned and primed steel, and the final painted condition



Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

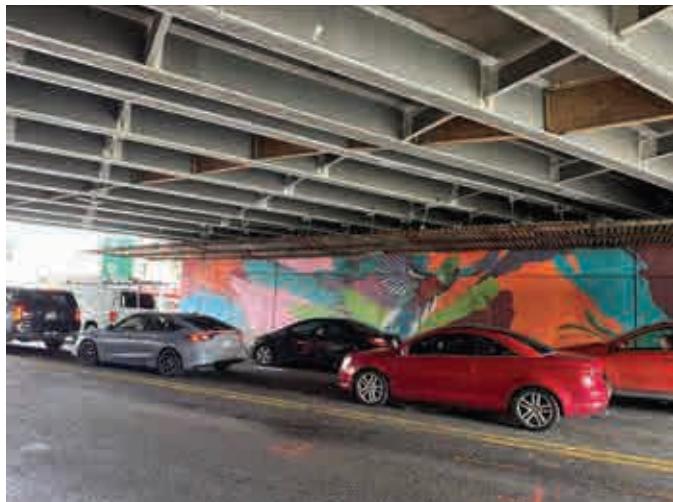
Route 1 over Webster Avenue – Chelsea



Webster Avenue – before condition



Webster Avenue – preparing for clean/paint



Webster Avenue – cleaned and primed



Webster Avenue – final condition after paint

Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

Project Progress Photographs

Paint chip site walk operations are conducted weekly for the streets surrounding the project site.



Lower Roadway barrier replacement – NB left shoulder shown



Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

Project Progress Photographs

Representative steel repairs and concrete repairs throughout project



Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

Project Progress Photographs

Representative containment set ups throughout the year



Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

A peer review was not performed prior to procurement.

Value Engineering

A Value Engineering (VE) Study of the project design was not performed.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues.

Extra Work Orders

The following represents extra work orders that have been processed, or will soon be processed, for the project.

EWO #1 – Clean and Paint Route 1 over Webster Avenue Bridge (C-09-015-3A5)

\$4,436,100 (substantially complete)

This simple span bridge is located approximately 1 mile north of the project and was cleaned and painted over an accelerated 6 week period during the summer/early fall.

EWO #2 – Northbound Permanent Roadway Barrier Replacement

\$789,712 (partially complete)

The northbound roadway at the northern limits of this project near the Fourth Street offramp has approximately 300 feet of substandard temporary concrete highway barrier that is deteriorating. The work is partially complete since it is time bound to the traffic control on the project.

EWO #3 – Tobin Bridge Electrical Feed

\$587,521 (anticipated 2026)

Project Name: Structural Cleaning & Painting, Steel & Concrete Repairs on Tobin Bridge, B-16-017

Owner's Representative Contract Number: 120633

Project Number: 605959

Construction Contract Number: 122602

The electrical equipment for the Tobin bridge is aging and deteriorating. This work is replacing critical switch gear components to continue to provide power to the bridge. The work is going to be authorized in December 2025.

EWO #4 – Beacon Ramp Highway Lighting Restoration

\$135,883 (anticipated 2026)

The light luminaries along Beacon Street off-ramp and not functional and this EWO is repairing them. The work is going to be authorized in December 2025.

OWNER'S REPRESENTATIVE OATH

I John F. Watters, MA P.E. No. 40751, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120633, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



John F. Watters, P.E.

Date: December 15, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative Contract No. 120630 - Allegro Construction

Services

Project No. 612681 – Tunnel Lighting Replacement on I-90 (CRC 17H & CRC 17I)

BOSTON, MASSACHUSETTS

Final Designer: WSP

Constr. Contractor: Dagle

Electric Construction Corp.

(DEC)

Prepared by: Allegro Construction Services

December 12, 2025

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I. EXECUTIVE SUMMARY

The scope of the I-90 Tunnel Lighting Replacement Project (MassDOT Project #612681) includes the removal and replacement of the tunnel lighting system in the I-90 Connector tunnels in the section between the Prudential Tunnel and the Ted Williams Tunnel. The existing linear fluorescent / low pressure sodium lighting will be replaced with a new LED tunnel lighting and control system. The project purpose is to provide adequate visibility in the tunnel for driver safety while using the least amount of energy. MassDOT is advancing this project using the Design-Bid-Build (DBB) method of project delivery. WSP is the project's Engineer of Record (EOR), tasked with developing the final design and procurement documents, and providing construction phase services. Dagle Electrical Construction Corp. is the contractor.

The lighting system is being replaced due to a series of defects including corrosion due to dissimilar metals, detached connection clips, broken butterfly and lens clips, wireway problems, exposed epoxy anchor sleeves, bent hanger rods, and other miscellaneous issues.

In 2022, MassDOT had been advancing two separate projects: 609318 for I-90 Eastbound, and 609343 for I90 Westbound. 609318 had advanced to the PS&E level, while 609343 had been advanced to the 100% design level. Balancing issues such as funding, procurement, and traffic impacts, MassDOT combined those two projects into one (now known as PI #612681). That combined project PS&E was advertised in June 2023 and Dagle was awarded the project, with NTP for this construction contract #122343 on October 5, 2023. In 2024, Dagle has been advancing construction, including procurement and installation of lighting systems and fixtures. As of the end of 2024, DEC completed 28% of the work (\$26.1 Million).

In 2025, DEC advanced the construction further. As of the date of this report, DEC has completed 60.4% of the work (\$59.8 Million)

The contract encumbrance has been set up at \$106,050,506 (including the bid price of \$88,744,788). Current estimated cost to complete is \$98,966,609. Funding for this project will come, in part, from the Central Artery Repair and Maintenance (CARM) Trust Fund, as well as from MHS funding.

The contract completion milestone is for a Field Completion date of 6/01/27. As of the date of this report, the Contractor is forecasting a delay of -231 days.

II. ANNUAL REPORT

Contract Scope of Work

The delivery of this project is by the traditional Design-Bid-Build procurement method. MassDOT has assigned WSP as the designer; they will act as project Engineer of Record (EOR). WSP also provides construction phase services.

The physical scope of this project is:

- Remove existing fluorescent luminaires, wireways, and wiring
- Install structural supports
- Install new LED lighting fixtures, raceways, and junction boxes
- Install and calibrate new lighting control system
- Install new power feeders at VB 8 for five jet fans in tunnel
- Manage traffic by using lane closures (i.e., maintain traffic during construction)

The limits of this project include the I90 tunnel system between the Prudential Tunnel and the Ted Williams Tunnel. The roadways included in this are I90 Westbound, I90 Eastbound, and I90 HOV-Eastbound. In addition, the following ramps are included: Ramps B, D, DN, W-SS, A, C, F, I, L and L-CS.

The various responsible project stakeholders are:

- MassDOT Highway Division – project owner, also providing project management; note that MassDOT is also considered the AHJ (Authority Having Jurisdiction) regarding tunnel code compliance issues;
- MassDOT District 6 – responsible for design and construction oversight (supported by various MassDOT technical departments);
- WSP – final designer / Engineer of Record;
- Dagle Electrical Construction Corp. (DEC) – the construction contractor responsible to procure and install the new lighting system

Major Progress as of December 1, 2025

The bulk of 2022 was spent in the design and consolidation of the two previously advanced projects into one combined project, with the PS&E submitted June 30, 2022. The remainder of the year was spent by MassDOT performing its review, and the Peer Review being performed by Mott McDonald.

The work in 2023 consisted of the completion of, and reconciliation of, the peer review followed by the resubmitted PS&E and the contract procurement process. Ultimately, the contract was awarded to Dagle, and an NTP was achieved in October 2023. Following NTP, Dagle mobilized and began the construction phase.

In 2024, DEC continued construction. As of the end of 2024, DEC completed approximately 28% of the work. Due to delays in procurement of wire, DEC was at that time forecasting a delay of 108 days.

In 2025, DEC continued working on the project, working during nighttime lane closures on the roadways and ramps and daytimes in the buildings. The field work included work in Ramps LC-S, A, D, HOV, and I90 EB as well as work in Vent Buildings 1, 5, and 8. That work consisted of removal of existing lights, installation of temporary lights, installation of raceway and junction boxes, installation of light fixtures, and wiring. As of the end of 2025, DEC completed approximately 60.4% of the work. Additional delays, including some imposed by lack of ramp access, have resulted in a forecasted delay of -231 days.

Budget

In accordance with the contract award, the project budget has the following breakdown:

<i>ITEM</i>	<i>TOTAL</i>
Bid	\$ 88,744,788
Allowance	\$ 3,994,000
Contingency	\$ 13,311,718
TOTAL	\$ 106,050,506

As of the date of this report, the contractor has been paid \$59.8 Million. MassDOT is funding this work from the Central Artery Repair and Maintenance (CARM) Trust Fund in addition to MHS funding to make up for the required state funds.

Schedule / Project Milestones

The project is advancing with the following expected key milestone:

Contractor Field Completion 6/01/27

In its November 2025 update, DEC is projecting a delay of 231 days but is working with MassDOT to develop methods of mitigating that delay.

Areas and Issues of Concern

- Traffic Impacts – the work is performed during lane closures, and this project includes many roadways and ramps. Other MassDOT work is ongoing in these roadways concurrently. For periods of time during 2025, DEC's access to certain ramps was negatively impacted and caused some delay. MassDOT and the contractor must continue to carefully plan and coordinate (and document) to ensure the traffic impacts are minimized.
- Schedule delay – MassDOT is working closely with DEC to assist in developing methods of mitigating the current projected delay.

Project Photographs

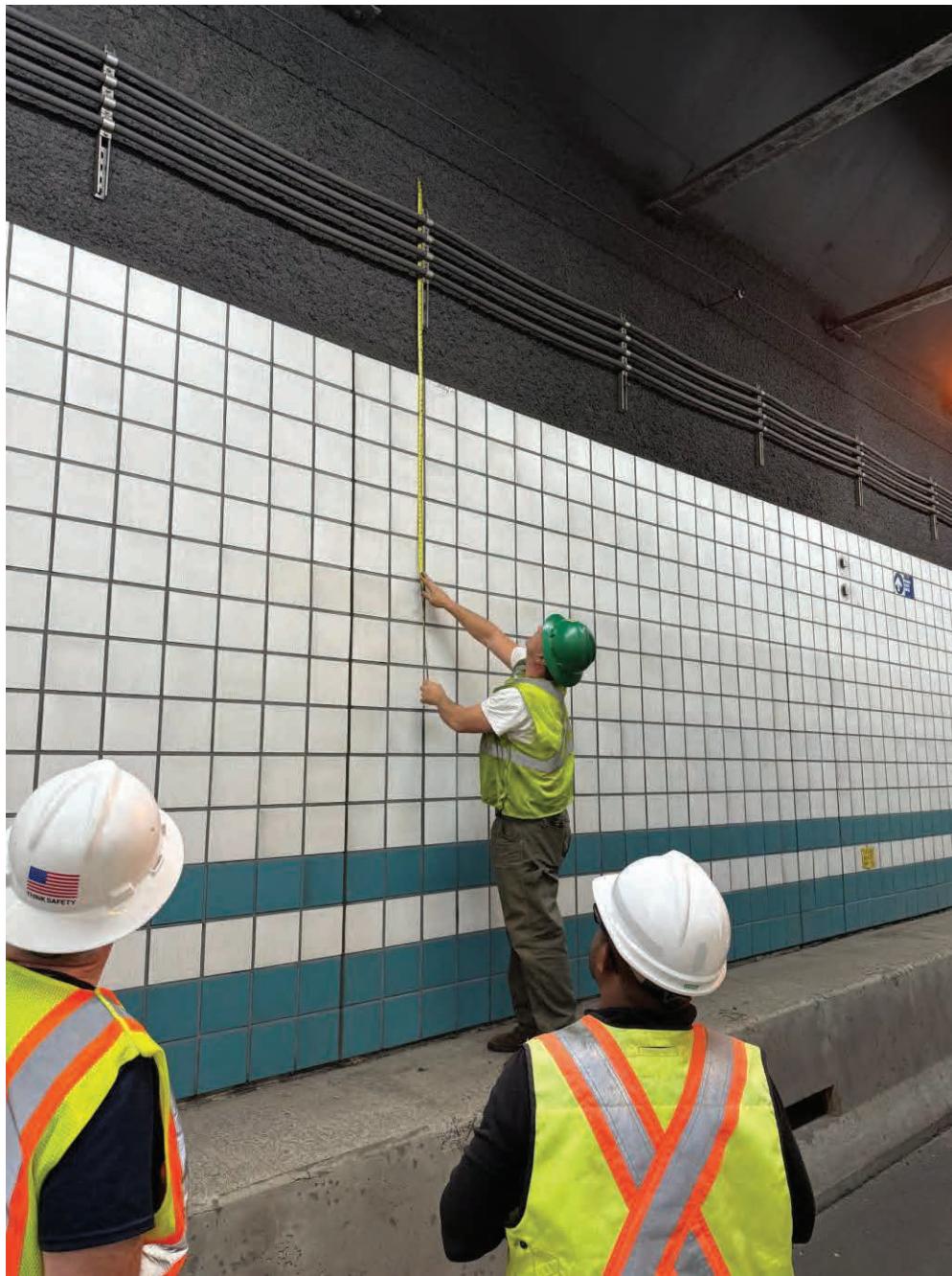
PHOTO ONE (below): Photo of installation of lighting in the I90 HOV EB Tunnel. The ongoing installation of permanent lighting, shown in left side of photo, will replace the overhead existing lighting.



PHOTO TWO (below): Photo of installation of lighting in the I90 Ramp F Tunnel. The permanent lighting on the wall will replace the overhead existing lighting.



PHOTO THREE (below): Photo of work in I90 WB – workers are measuring to develop workaround to install wall lighting without conflicting these existing conduits.



III CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

The peer review requirement for this project was satisfied by MassDOT's use of Mott McDonald to perform this service. As of the date of this Annual Report, Mott McDonald's peer review is complete, and the results were reconciled with MassDOT and WSP before the final design package was advertised.

During the construction phase of this project, WSP, as EOR, is continuing to provide construction phase services. In addition, this O.R. will continue to partially satisfy the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and monitoring the change order process to ensure any proposed design changes are evaluated for their impacts to the project's quality, budget, or schedule goals.

Value Engineering

MassDOT did not perform a traditional Value Engineering study, due to the nature of this project. It is important to note that this project is one of a series of projects developed to replace the lighting system throughout the CA/T tunnels. Several projects have been completed or are nearly complete. The technical solution for this project must essentially follow those previous projects, as MassDOT does not want to have dissimilar systems.

Cost Recovery

At this time no items have been identified as potential cost recovery issues.

Extra Work Orders

As of the date of this report there have been two EWOs accepted, for a total of \$788,222.92; and one EWO accepted pending final approval.

Project Name: Tunnel Lighting Replacement on I-90 - Boston
Owner's Representative Contract Number: 120630
Project Number: 612681
Construction Contract Number: 122343

- EWO 1: Provide Cisco switches to be consistent with MassDOT system - \$168,958.42
- EWO 2: Provide 13 new light pole fixtures, 4 new cameras, remove and reset 4 (ITS) poles, provide 4 new pole foundations for the (ITS) poles and repair the existing load center enclosure in East Boston - \$619,264.50
- EWO 3: Provide new updated lighting system at various locations along the Greenway in Boston - \$3,917,408.55

Cost – the estimated budget of \$106 M needs to continue to be monitored during construction, especially the status of contingency. At this time the budget appears to be adequate for the remaining work.

Schedule - the contract requires completion by June 2027. As of November 2025, the contractor is projecting a delay of up to 231 days due to delays in its procurement of wire as well as due to access delays in some of the ramps.

OWNER'S REPRESENTATIVE OATH

I, Joseph J. Allegro, Jr., MA P.E. No. 38723, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120630, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



Owner's Representative

Joseph J. Allegro, Jr., P.E.

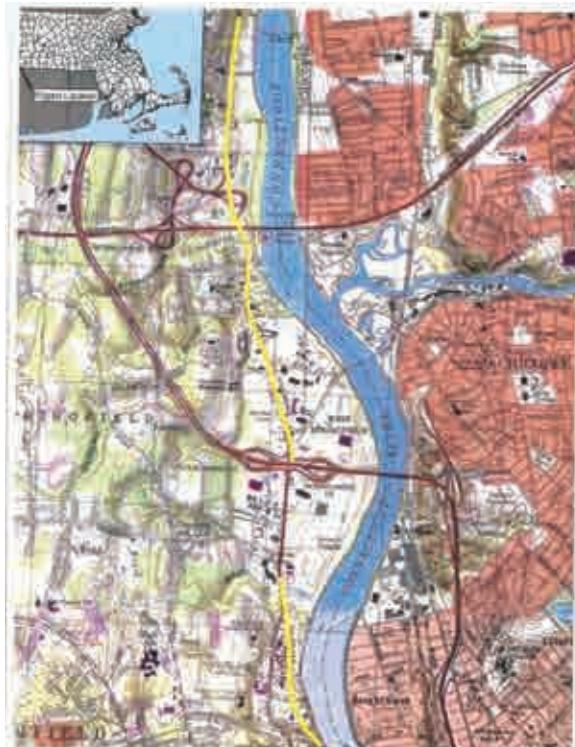
Date: December 12, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Holyoke-West Springfield
Rehabilitation of Route 5 (Riverdale Road)
Project No. 604209
Contract No. 123409



Owner's Representative's 2025 Annual Report

OR Contract No.: 120631 Fuss & O'Neill

Project No. 604209 HOL YOKE-WEST SPRINGFIELD (Rehabilitation of Route 5 (Riverdale Road))

Contractor: Ludlow Construction Co., Inc.

Designer: HNTB

Prepared by: Fuss & O'Neill/Bayside Engineering, Inc.

December 16, 2025

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EXECUTIVE SUMMARY

This corridor rehabilitation project is along the Route 5 Corridor (Riverdale Street) and is essentially split in to two distinct segments having differing work items. (Please refer to page 4 for a drawing of the two specific work areas.) The first section begins at the ramp system to Route 91 in West Springfield and runs northward for approximately 2.42 miles to a short distance north of its intersection with Main Street in Holyoke. The work in this section includes concrete pavement rehabilitation and full depth replacement along Route 5.

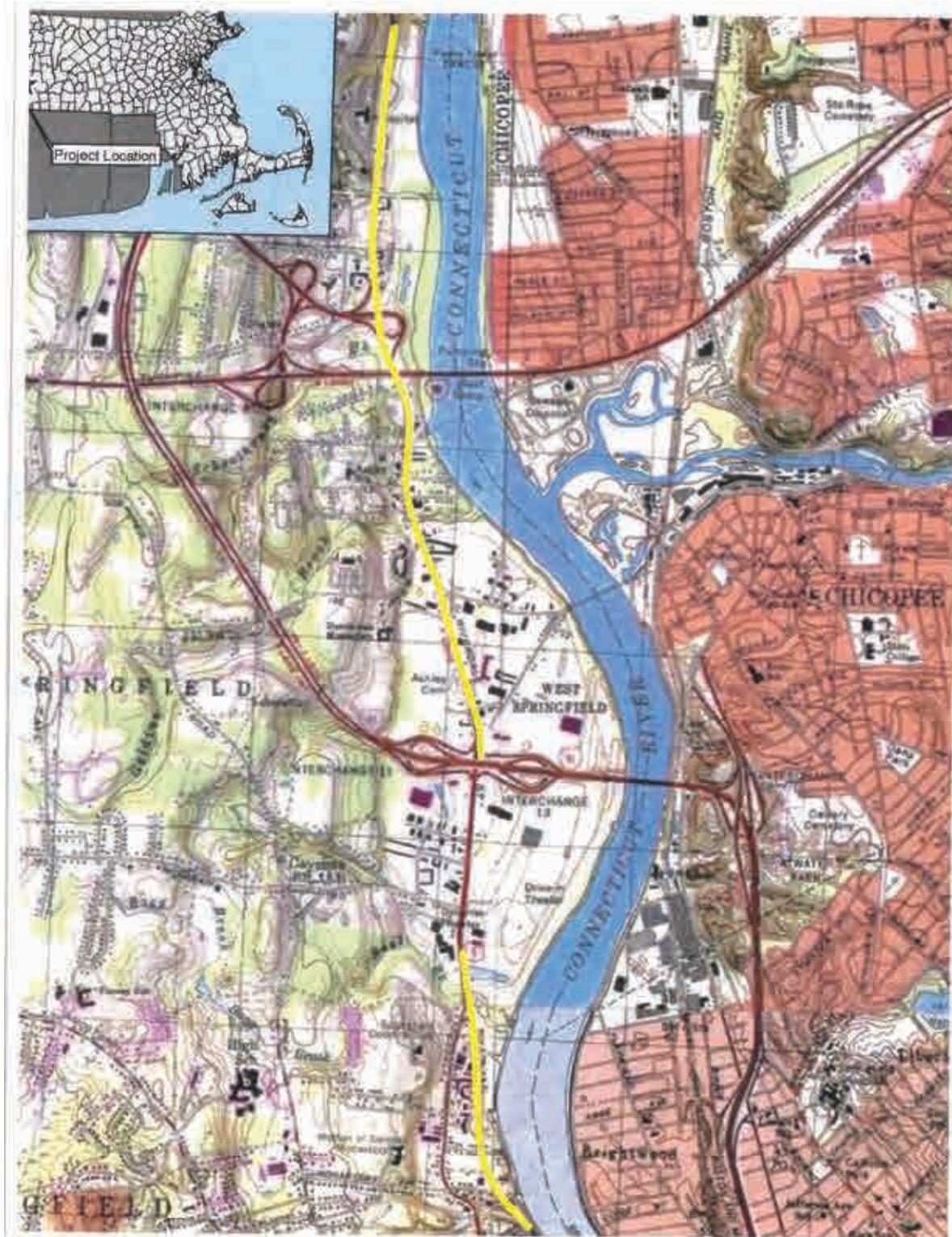
The second section begins approximately 0.64 miles south of the beginning of section one above and runs southerly along Route 5 in West Springfield from its intersection with Monterey Street to its intersection with East Elm Street, a distance of approximately 0.92 miles. Work in this section consists primarily of pedestrian and bicycle improvements.

The proposed improvements consist of narrowing the roadway from Main Street to Highland Avenue, realignment of Brush Hill Avenue and Wayside Avenue to form “T” intersections, traffic improvements, shared use path on the western side of the northern section of the project, bicycle accommodations for both directions of travel, drainage improvements, and utility modifications.

The roadway work is comprised of concrete pavement rehabilitation, full depth cement concrete pavement, pavement narrowing, excavation, borrow, grading, installation of hot mix asphalt pavements, installation of curb and sidewalk, removing and replacement of highway guard rails, pavement markings, traffic signal modifications, catch basins, and other incidental work.

As part of this project, the Town of West Springfield is upgrading their water lines under non-participating items funding and Eversource is replacing its natural gas lines. Verizon will also be upgrading their system.

HOLYoke-WEST SPRINGFIELD Rehabilitation of Route 5 (Riverdale Road)
Project No. 604209, Contract No.123409



Map Showing Limits of Work In Yellow (Two Distinct Sections)

ANNUAL REPORT

Contract Scope of Work:

The proposed improvements consist of narrowing the roadway from Main Street to Highland Avenue, realignment of Brush Hill Avenue and Wayside Avenue to form “T” intersections, traffic improvements, shared use path on the western side of the northern section of the project, bicycle accommodations for both directions of travel, drainage improvements, and utility modifications.

The roadway work is comprised of concrete pavement rehabilitation, full depth cement concrete pavement, pavement narrowing, excavation, borrow, grading, installation of hot mix asphalt pavements, installation of curb and sidewalk, removal and replacement of highway guard rails, pavement markings, traffic signal modifications, catch basins, and other incidental work.

As part of this project, the Town of West Springfield is upgrading their water lines under non-participating items funding. Eversource gas is relocating some lines due to roadway construction conflicts and Verizon and Eversource are moving poles due to conflicts as well.

Major Progress as of November 30, 2025

The Contractor installed drainage throughout the corridor, Eversource along with Verizon moved their poles, and Eversource continued upgrading their gas lines. In addition to the drainage installation, the Contractor pretty much finished the southern end of the project. This included new curbing, concrete sidewalks, and a hot mix asphalt shared use path.

Budget:

Bid Price: \$45,279,109

Estimated Contract Cost: \$46,283,109

Encumbered* Amount: \$50,812,064

*Includes Contingencies, Railroad Flaggers, Traffic Police, Specialty Services, and Test Pit Excavations.

Total Amount Expended** through Nov. 1, 2025: \$14,801,456 of which \$4.2M is for Town Included Waterline Replacement.

Percent Completion to Date: 26.34

Current Estimate at Completion: \$47.829,894

Schedule:

The Contractor was issued a Notice to Proceed on December 5, 2023 but was unable to start physical construction until April 2024 due to required submittals. The latest Schedule Update 22 indicates that the Contractor Field Completion is 334 days behind schedule and is currently forecast to be on July 7, 2028. The Contractor attributes this delay to continued waiting for the concrete panel survey to be produced that indicates which panels can remain and which ones need replacing.

Another delay is due to a final disposition determination regarding the directional drilling of the water service connections. It appears the designer for the Town of West Springfield did not take into account the amount of private property easements for testpits and for the waterline directional drilling process. Recent developments indicate that the Town is willing to have the service connections installed by standard excavation methods, and thus additional costs will be incurred.

Full depth excavation is now not expected to begin until spring of 2026.

Areas and Issues of Concern

Aside from the project schedule delays discussed above the only minor concern at this point is the actual placement of the cement concrete pavement which has not been done is decades and will require extra attention by the Contractor.

Project Photographs

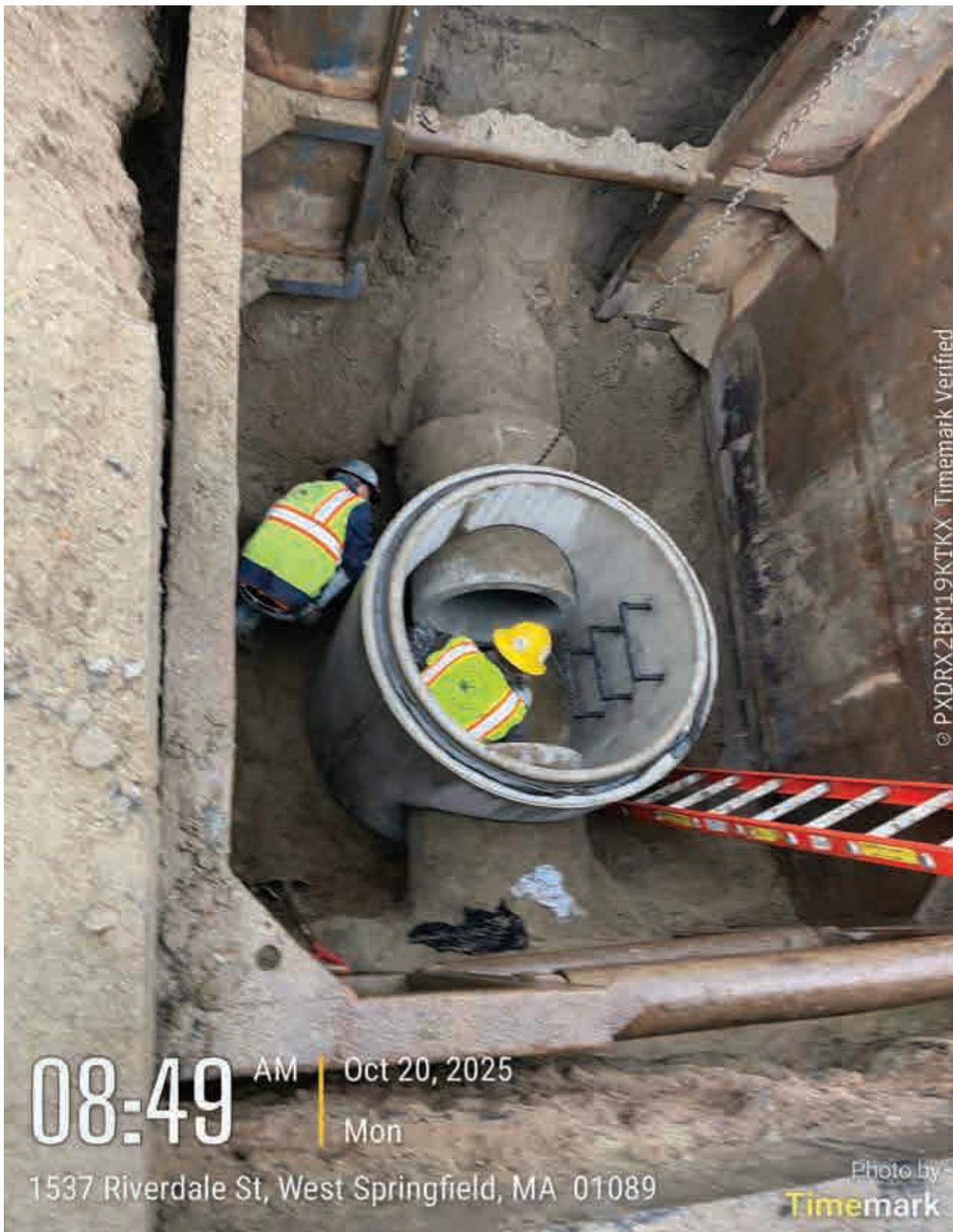


Placing Cement Concrete Sidewalk at Southern End of Project

HOLYoke-WEST SPRINGFIELD Rehabilitation of Route 5 (Riverdale Road)
Project No. 604209, Contract No.123409



Completed Sidewalks at Driveways



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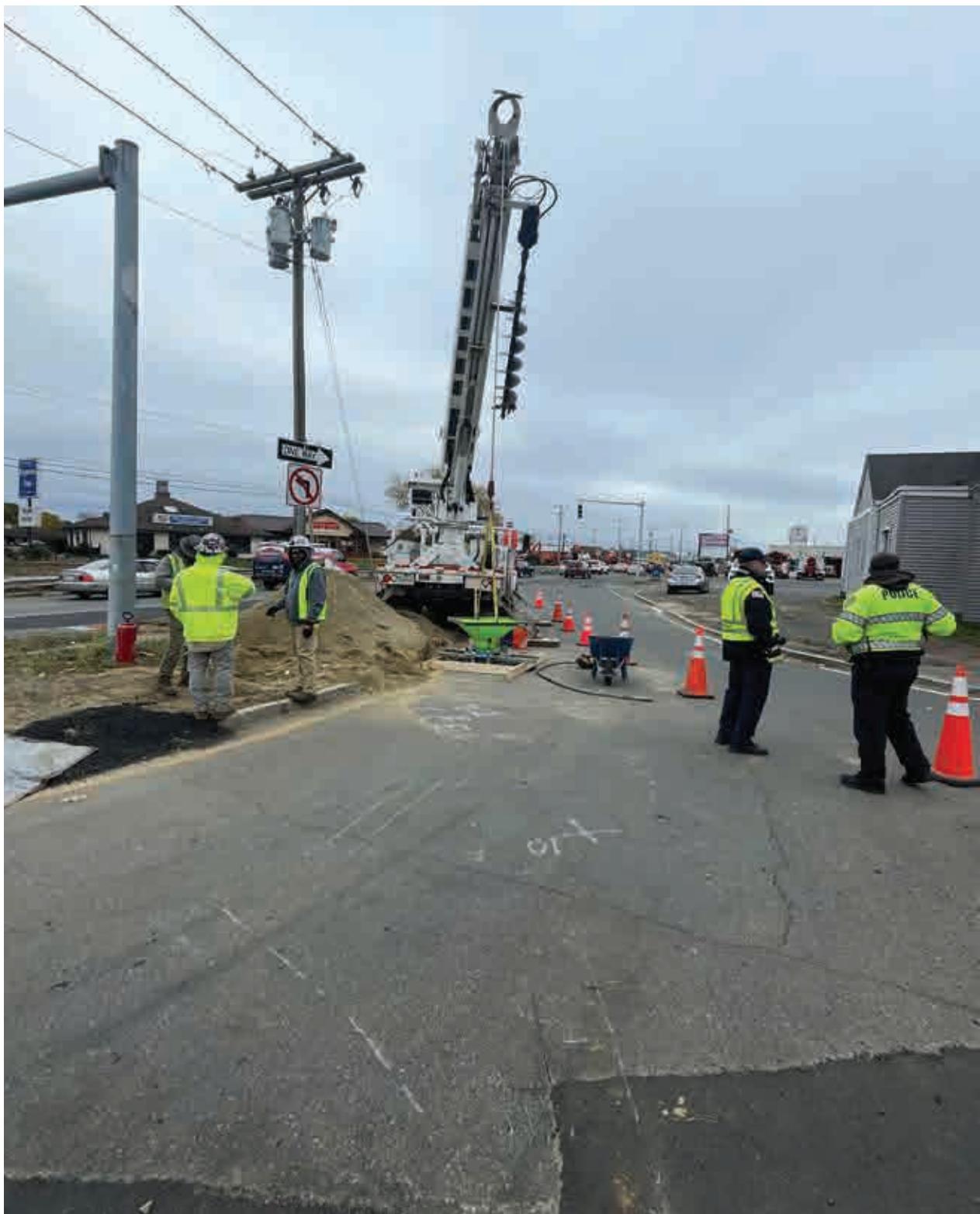
Mon

1537 Riverdale St, West Springfield, MA 01089

Photo by
Timemark

Continued Drainage Installation

HOLYKE-WEST SPRINGFIELD Rehabilitation of Route 5 (Riverdale Road)
Project No. 604209, Contract No.123409



Prepping Augured Hole for Mast Arm Foundation Concrete

HOLYKE-WEST SPRINGFIELD Rehabilitation of Route 5 (Riverdale Road)
Project No. 604209, Contract No.123409



Conduit Installation for Traffic Signal

CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

As part of the initial design phase for this project, evidence supports that a peer review was performed at each stage of the design by MassDOT's Highway Division forces independent of the project designer. As the design process developed, Addenda was issued, primarily involving better defining the final product desired. These reviews were performed to identify conflicts or errors that should be corrected or mitigated at each stage before the design progressed to the next stage.

During the construction phase of this project, this O.R. is satisfying the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

Value Engineering

It is the Owner's Representative's understanding that no Value Engineering process was utilized on this Project.

Cost Recovery

At this early stage of the project, nothing has occurred that can be identified as warranting seeking Cost Recovery. It should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's Representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for its referral to the Cost Recovery Standing Committee. For any issue referred for Cost Recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 8/7/17) shall govern. In order to perform the legislative

HOLYoke-WEST SPRINGFIELD Rehabilitation of Route 5 (Riverdale Road)
Project No. 604209, Contract No.123409

mandate of being the Primary Manager of Cost Recovery, the Owner's Representative will follow the referenced SOP. This SOP, as modified for projects with an Owner's Representative, describes the role of the Owner's Representative and includes a Cost Recovery Standing Committee. This committee oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary.

OATH

I, Norman H. Brown, P.E., MA P.E. No. 36531, hereby certify that my sole responsibility as Owner's Representative, under OR Contract 80661, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

A handwritten signature in blue ink that reads "Norman H. Brown". The signature is cursive and fluid, with "Norman" on the first line and "H. Brown" on the second line.

Norman H. Brown, P.E., P.L.S.

Date: December 16, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



OWNER'S REPRESENTATIVE 2024 ANNUAL REPORT

Owner's Representative Contract No. 124530 – HDR Engineering Inc.

***Project No. 609346 – Boston-Tunnel Lighting Replacement for I-93 (NB/SB) Ramps I-93 Project
BOSTON, MASSACHUSETTS***

Final Designer: WSP

Constr. Contractor: JF White Construction Co. (JF White)

Prepared by: HDR

November 25, 2025

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I. EXECUTIVE SUMMARY

The scope of the I-93 Tunnel Ramps Lighting Replacement Project (MassDOT Project Information #609346) includes the removal and replacement of the tunnel lighting system in the I-93 Ramps and Vent Buildings. The existing linear fluorescent / low pressure sodium lighting will be replaced with a new LED tunnel lighting and control system. The project purpose is to provide adequate visibility in the tunnel for driver safety while using the least amount of energy. MassDOT is advancing this project using the Design-Bid-Build (DBB) method of project delivery. WSP is the project's Engineer of Record (EOR), tasked with developing the final design and procurement documents, and providing construction phase services. JF White is the contractor.

The lighting system is being replaced due to a series of defects including corrosion due to dissimilar metals, detached connection clips, broken butterfly and lens clips, wireway problems, exposed epoxy anchor sleeves, bent hanger rods, and other miscellaneous issues.

II. ANNUAL REPORT

Contract Scope of Work

The delivery of this project will be by use of the traditional Design-Bid-Build procurement method. MassDOT has assigned WSP as the designer; they will act as the project Engineer of Record (EOR). When ready, the project will be advertised for construction, and WSP will provide construction phase services.

The work under this Contract includes furnishing and installing new LED tunnel luminaires in the I-93 Ramps Tunnel Lighting Rehabilitation. Work includes upgrading electrical, control and structural systems associated with the new tunnel lighting system and includes the following:

- Remove existing luminaires and wireways as described in the Special Provisions.
- Remove existing wiring within the wireways and raceways.
- Furnish and install new structural supports for tunnel luminaires for each mounting type as shown in the Contract Plans.
- Furnish and install new LED tunnel luminaires in accordance with Manufacturer's installation requirements.
- Install new electrical raceways and boxes for the new lighting system including separate raceways for emergency and normal power.
- Install tunnel lighting control system as described in the Special Provisions and as shown in the contract Plans.
- Perform tunnel lighting control system calibration according to the Manufacturer's requirements.
- Include any and all required hardware for raceways, including nuts, washers, bolts, spring nuts, and other hardware.
- Repair and/or provide any and all required Fireproofing within the tunnels.

The limits of this project include the I93 tunnel ramp system and vent buildings. The project is broken up into two phases and phase I work needs to be completed prior to beginning phase II work. The phase I work contains light replacements in Ramp R-T Ramp A-CN Ramp A-CN/R-T Ramp CN-SA Ramp SA-CN Ramp ST-SA/CN Ramp ST-S I-90 Collector Ramp R-S Ramp R-R Ramp CS-P Ramp SA-CT Ramp CS-SA/CT Ramp SA-CS Ramp CS-SA Air Intake Structure Vent Building 3 Vent Building 4 and phase II work contains light replacement in Ramp CN-S Ramp C Ramp D-N Ramp S-N Vent Building 8 Vent Building 1

The various responsible project stakeholders are:

- MassDOT Highway Division – project owner, also providing project management; note that MassDOT is also considered the AHJ

(Authority Having Jurisdiction) regarding tunnel code compliance issues;

- MassDOT District 6 – responsible for design and construction oversight (supported by various MassDOT technical departments);
- WSP – final designer / Engineer of Record;
- Mott MacDonald – independent consultant to MassDOT, performing Peer Review services; and
- JF White Construction Company (JF White) – the construction contractor responsible to procure and install the new lighting system

Major Progress as of November 25, 2025

The bulk of 2025 was spent by JF White in getting the temporary lighting, installed and operational. Work progress summary as of November 25, 2025:

Ramp A-CNR-T:

Temporary Lighting – 100% complete

Demolition – 100% complete

Conduit & Junction Boxes – 80% complete

Lights – 28% complete

Ramp CS-P:

Temporary Lighting – 100% complete

Demolition – 100% complete

Supports – 95% complete

Ramp A-CN:

Temporary Lighting – 100% complete

Demolition – 100% complete

Supports – 99% complete

Ramp R-R:

Temporary Lighting – 100% complete

Demolition – 100% complete

Supports – 95% Complete

Conduit & Junction Boxes – 5% complete

Ramp R-T:

Temporary Lighting – 100% complete

Demolition – 100% complete

Supports – 30% Complete

Conduit & Junction Boxes – 18% complete

Ramp R-S:

Temporary Lighting – 100% complete

Demolition – 100% complete

Conduit & Junction Boxes – 90% complete

Lights – 80% complete

I-90 Collector:

Temporary Lighting – 100% complete

Demolition – 100% complete

Conduit & Junction Boxes – 25% complete

Supports – 60% complete

Ramp S-N:

Temporary Lighting – 60% complete

Ramp CN-S:

Temporary Lighting – 40% complete

Ramp SA-CN:

Temporary Lighting – 100% complete

Demolition – 100% complete

Supports – 3% Complete

Ramp SA-CS:

Temporary Lighting – 100% complete
Demolition – 100% complete
Supports – 40% Complete
Conduit & Junction Boxes – 40% complete
Lights – 19% complete

Ramp ST-S:

Temporary Lighting – 100% complete
Demolition – 100% complete
Supports – 40% Complete
Conduit & Junction Boxes – 40% complete
Lights – 22% complete

Ramp CS-SACT:

Temporary Lighting – 100% complete
Demolition – 100% complete
Supports – 50% Complete
Conduit & Junction Boxes – 40% complete
Lights – 12% complete

Ramp ST-SA/CN:

Temporary Lighting – 100% complete
Demolition – 96% complete
Supports – 1% Complete

Ramp SA-CT:

Temporary Lighting – 100% complete
Demolition – 100% complete
Supports – 25% Complete

Conduit & Junction Boxes – 25% complete

Lights – 15% complete

Ramp CN-SA:

Temporary Lighting – 100% complete

Demolition – 60% complete

Supports – 48% Complete

Conduit & Junction Boxes – 40% complete

The Contractor has a few ramps that they have not begun to work on due to access constraints.

They also have work in the vent buildings that they have begun working in but have not completed the work within them.

Budget

In accordance with the contract award, the project budget has the following breakdown:

<i>ITEM</i>	<i>TOTAL</i>
Bid	\$ 58,326,165
Allowance	\$ 3,138,000
Contingency	\$ 8,748,925
TOTAL	\$ 70,213,090

As of the date of this report, the contractor has been paid up to pay estimate 17 in the total \$18,052,599.63 for approximately 26% of the fee completed to date.

As of the date of this report, MassDOT anticipates being able to use funding from the Central Artery Repair and Maintenance (CARM) Trust Fund for the bulk of the funding for the project, in addition to MHS funding to make up for the required state funds.

The current estimated % work completed by the Contractor is 38%

Schedule / Project Milestones

The project is advancing with the following expected key milestones:

Original Contract Completion Date	10/12/28
Current Contract Completion Date	10/15/28
Contractor's Forecasted Completion Date	12/27/28

The current drive for the critical path is the Northbound ramp construction beginning with the installation of the structural supports at the ST-S ramp at the East and West walls.

The Contractor's forecasted completion date has not been fully accepted, and the schedules are being returned accepted with comments.

Areas and Issues of Concern

- Traffic Impacts – the work is intended to be performed during lane closures, and this project includes many roadways and ramps; the traffic impacts must be carefully planned and coordinated – both within this project and with other projects. There was a recent accident during work hours where a vehicle entered the work area prior to the state police being present at this location. MassDOT and Contractor has implemented using Type III barricades at this and other “high risk” locations throughout the project area.
- Quality control – in implementing similar projects (replacement of tunnel lighting) throughout the downtown Boston area, MassDOT has had the opportunity to see its QC processes evolve, including QC oversight at off-site manufacturing facilities. This topic will continue to be a very important focus for the project team. WSP and MassDOT has been performing hands on visual inspection of the off-site manufactured items upon arrival and prior to acceptance.

Project Photographs

April 2025 site visit



Figure 1 Showing installed lighting conduit and junction box

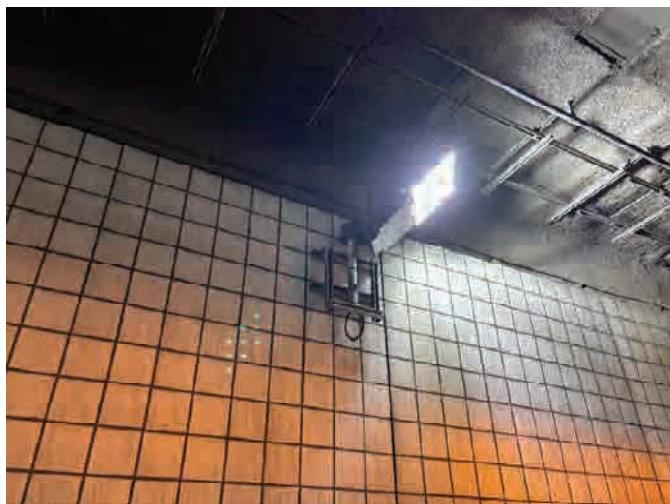


Figure 2 Showing installed temporary lighting

October 2025 site visit



Figure 3 Showing Contractor installing lighting conduits

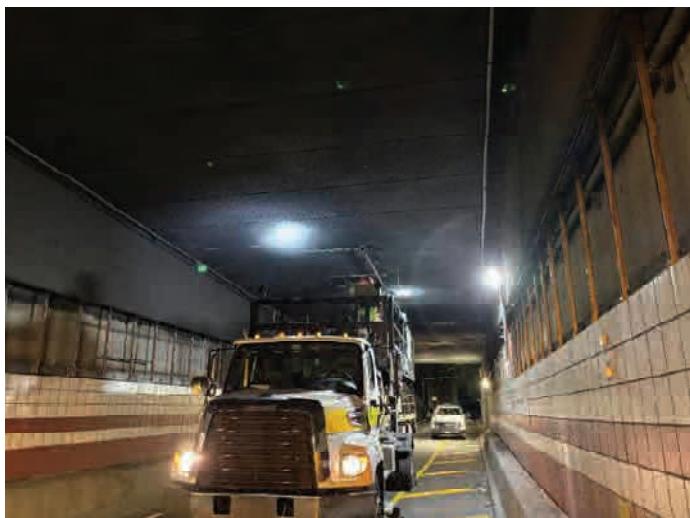


Figure 4 Showing Contractor installing lighting conduit supports

III CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

The peer review requirement for this project is satisfied by MassDOT's use of Mott MacDonald to perform this service. As of the date of this Annual Report, Mott MacDonald's peer review is complete, and the results were reconciled with MassDOT and WSP before the final package was advertised.

During the construction phase of this project, WSP, as EOR, will continue to provide construction phase services. In addition, this O.R. will satisfy the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and monitoring the change order process to ensure any proposed design changes are evaluated for their impacts to the project's quality, budget, or schedule goals.

Value Engineering

MassDOT did not perform a traditional Value Engineering study, due to the nature of this project. It is important to note that this project is one of a series of projects developed to replace the lighting system throughout the CA/T tunnels. Several projects have been completed or are nearly complete. The technical solution for this project must essentially follow those previous projects, as MassDOT does not want to have dissimilar systems.

Another potential candidate for value engineering is the wireway. MassDOT performed a pilot test to study whether the wireway could remain in place; unfortunately, that study determined the wireway had to be replaced.

Cost Recovery

At this time no items have been identified as potential cost recovery issues.

Extra Work Orders

As of the date of this report there have been no EWOs accepted.

Cost – the estimated budget of \$70 M needs to be monitored during construction, especially the status of contingency.

Schedule - the project duration was established by MassDOT via the use of an independent

consultant's time determination schedule, with a reconciled completion schedule. The final milestone dates were reflected in the bid package. The contractor has been submitting monthly schedule updates for review and approval by WSP and MassDOT.

OWNER'S REPRESENTATIVE OATH

I, Charles H Swanson, MA P.E. No. 40907, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 124530, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.

A handwritten signature in black ink, appearing to read "Charles H. Swanson".

Owner's Representative

Charles H Swanson, P.E.

Date: December 1, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and
Interchange Improvements in Natick

Owner's Representative Contract Number: 120640

Project Number: 605313

Construction Contract Number: 125520



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative: Contract No. 120640 Weston & Sampson Inc.

Project No. 605313: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over

Route 9 (Worcester Street) and Interchange Improvements

Greenman-Pedersen, Inc.

Design-Builder: McCourt Construction Company, Inc.

Jacobs Engineering Group, Inc. (Engineer of Record)

Prepared by: Mark King P.E., Weston & Sampson, Inc.
December 15, 2025

Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and
Interchange Improvements in Natick
Owner's Representative Contract Number: 120640
Project Number: 605313
Construction Contract Number: 125520

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Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and Interchange Improvements in Natick
Owner's Representative Contract Number: 120640
Project Number: 605313
Construction Contract Number: 125520

I. EXECUTIVE SUMMARY

The Natick Bridge Replacement, N-03-020, Route 27 (North Main Street) over Route 9 (Worcester Street) and Interchange Improvements is a design-build project to replace the structurally deficient bridge carrying Route 27 (North Main Street) over Route 9 (Worcester Street) in the town of Natick, Massachusetts, with new bridges designed for a 75-year design life and improvements within the interchange by designing a modified diverging diamond configuration.

As part of this Project, the existing Route 27 (North Main Street) bridge over Route 9 (Worcester Street) will be replaced by three (3) independent structures (two structures for vehicles and one structure for a shared use path). This configuration was deemed to be highly desirable by the Natick community as part of the robust public engagement process. Providing an interchange that improves safety, mobility, and operations was of utmost importance. Equally important was an interchange configuration that was context sensitive and that would support Natick's quality of life.

The limits of roadway construction extend along Route 9 (Worcester Street) approximately from the Cochituate Rail Trail Bridge over Route 9 (Worcester Street) to Linden Street, a distance of approximately 0.74 miles. Within the limits of the Project, Route 9 (Worcester Street) has an ADT of 65,200 and a posted speed limit of 50 miles per hour. The Project includes median replacement, a cement concrete sidewalk along the north side of Route 9 (Worcester Street), a buffered HMA shared use path along the south side of Route 9 (Worcester Street), roadway milling and pavement overlay, and localized widening of Route 9 (Worcester Street) to accommodate improved acceleration and deceleration lanes in the Route 27 (North Main Street) interchange area. The work also includes roadway reconstruction of Route 27 (North Main Street) to construct a Modified Diverging Diamond Interchange consolidating Route 9 (Worcester Street) on-ramps and off-ramps to improve merge/diverge operations.

The limits of roadway construction extend along Route 27 (North Main Street) from approximately Running Brook Circle to Bacon Street, a distance of approximately 0.48 miles. Within the limits of the Project, Route 27 (North Main Street) has an ADT of 23,050 and a posted speed limit of 30 miles per hour. Work along Route 27 (North Main Street) includes roadway reconstruction and realignment, an HMA shared use path, and a cement concrete sidewalk.

Roadway work includes vertical and horizontal alignment adjustments, variable depth milling and

Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and Interchange Improvements in Natick

Owner's Representative Contract Number: 120640

Project Number: 605313

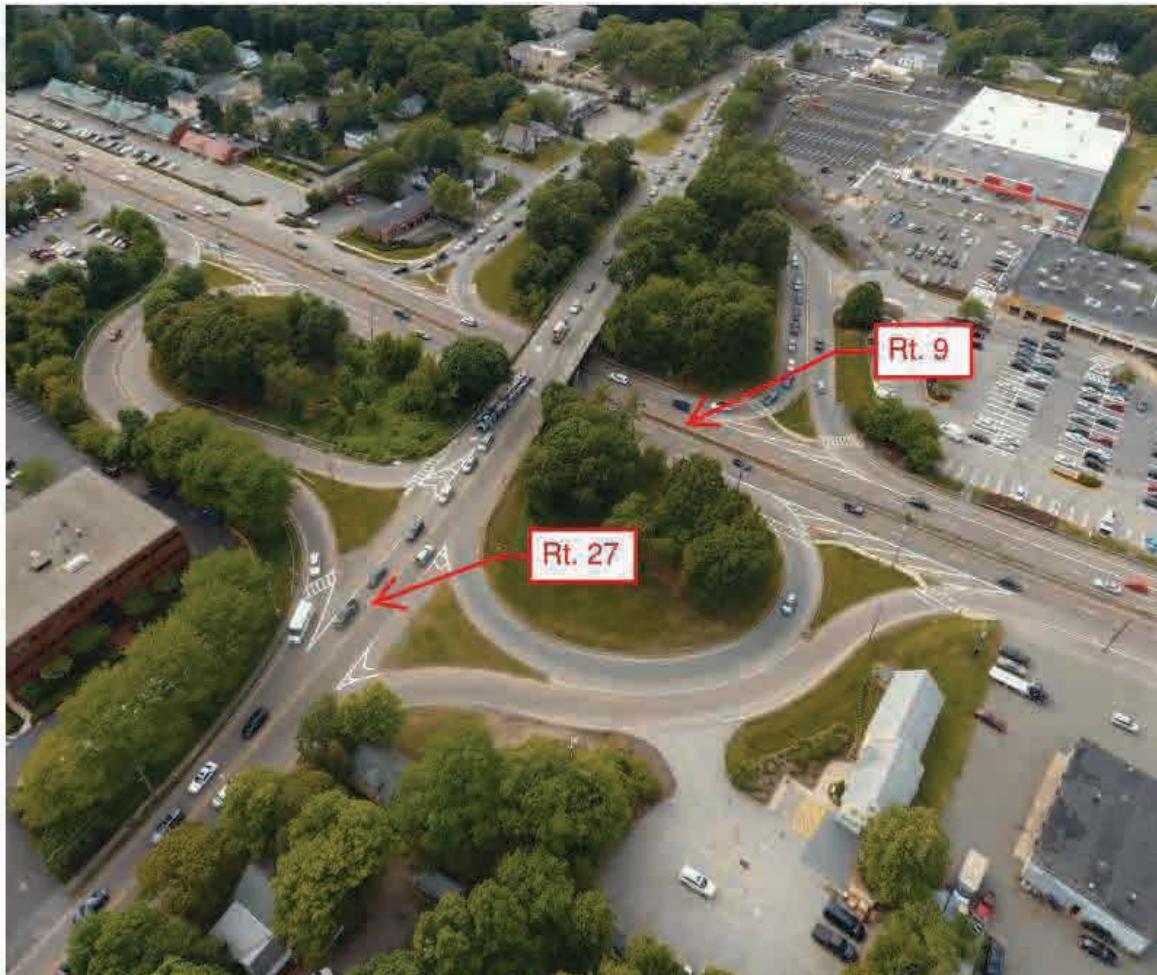
Construction Contract Number: 125520

HMA overlay; full-depth HMA construction, roadways for staged construction, grading, walls, granite curb, HMA shared use paths, cement concrete sidewalks, temporary and permanent drainage including detention ponds, highway lighting, temporary and permanent traffic signals, relocation of utilities, signage, pavement markings, and other related work.

A locus map of the study area is shown in Figure 1. This project is being constructed by the Massachusetts Department of Transportation (MassDOT) - Highway Division with Greenman-Pedersen, Inc. as the Preliminary Designer of Record. The project was procured as a Design-Build Contract with the Project awarded on August 22, 2024, with the apparent Best Value being the Design-Build Team of McCourt Construction Company Inc. of Boston, MA as the Prime Contractor and Jacobs Engineering Group Inc. as Design-Build Designer of Record. Notice to Proceed was issued to McCourt on September 19, 2024. In 2025, major progress includes utility relocation by Eversource (gas and electric) and other utilities, coordination with abutters, design package submissions, and the retaining wall at 190 N. Main St. under construction.

Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and Interchange Improvements in Natick
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Project Number: 605313
Construction Contract Number: 125520

Figure 1 - Project Location



Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and Interchange Improvements in Natick
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Construction Contract Number: 125520

II. ANNUAL REPORT

Contract Scope of Work

The Work includes but is not limited to:

Final Design and Construction of all elements.

- Advancing Highway design through 75%, 100%, and Issued for Construction Highway, and through Construction of all elements.
- Advancing Bridge design through Structural Bridge Submittals and Issued for Construction Structural and through Construction of all elements.
- Demolition of one (1) bridge.
- Construction of three (3) bridge structures, two (2) vehicular bridge and one (1) shared use path bridge.
- Roadway rehabilitation/reconstruction on Route 9 (Worcester Street) and Route 27 (North Main Street) including sections of full depth HMA pavement, pavement milling, and variable depth milling with an HMA overlay for development of proposed cross slopes.
- Geometric modifications to Route 9 (Worcester Street) including localized widening eastbound and westbound to accommodate improved acceleration and deceleration lanes in the Route 27 interchange area. Localized widening on Route 9 (Worcester Street) eastbound and westbound which will result in full-length acceleration and deceleration lanes to improve merge/diverge operations.
- Geometric modifications to Route 27 (North Main Street) to increase the roadway profile over Route 9 to achieve 16'-6" clearance and to incorporate a Modified Diverging Diamond Interchange design as depicted in the BTC Plans.
- Roadside and median barriers, guardrail, associated barrier end treatments, walls, fences and granite curb and edging.
- Temporary traffic control and management to maintain safe traffic operations and acceptable roadway conditions during staged construction.
- Design and construction of two new traffic signals along Route 27 (North Main Street) at the Modified Diverging Diamond crossovers and associated ramps, reconstruction of the existing traffic signal at the 9/27 Exchange driveway, and minor modifications/coordination with the existing traffic signal at the intersection with Bacon Street.
- Construction of all improvements and modifications to drainage systems for temporary and permanent conditions including infiltration areas, detention basins and swales.
- Removal and disposal of existing utility creosote wood ducts, multi-tile ducts, and other materials

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when encountered.

- Removal and disposal of decommissioned gas pipe, conflicting with proposed work, including coordination with Eversource to confirm the main is abandoned prior to cutting or removal.
- Construction of all signs and pavement markings and other traffic control devices, including overhead signs, guide signs and route markers, milled rumble strips, and slotted pavement markers.
- Landscaping and restoration of disturbed areas within the Project limits including identifying invasive plant species and limits, providing appropriate management and treatment plans and specifications and execution of management plan.
- Installation, maintenance, and removal of all erosion control protections required for work activities, including but not limited to sedimentation fence, sedimentation barrier, and silt sacks or other siltation and erosion control measures.
- Design and installation of lighting for illumination of temporary and permanent roadway lighting and temporary and permanent underdeck lighting.
- Design and installation of lighting for illumination of temporary and permanent pedestrian shared- use path lighting for connections between the proposed transit stops on Route 27 and Route 9.
- Constant monitoring, protection, and reinforcing (as necessary) of existing bridge elements to temporarily remain in service.
- Restoration of damaged pavement and disturbed areas within the Project limits.
- Coordination with private and municipal utility owners.
- Design and installation of underground infrastructure for electrical and communication relocations, including coordination to confirm design and materials meet any and all requirements of the utility owner.
- Coordination during design and construction with stakeholders, property owners, State/Municipal officials, local businesses, MetroWest Regional Transit Authority (MWRTA), and residents in conjunction with MassDOT.
- Off peak, night, and weekend work as required to facilitate demolition and construction.
- Design of temporary works to facilitate staged demolition of the existing bridge and construction of the proposed bridges.
- Coordination with surrounding private and public construction activities.
- Community outreach program in conjunction with MassDOT, including maintenance of the Project website to distribute current construction information, particularly as it relates to traffic impacts.
- Compliance with applicable regulatory approvals and permits issued prior to Notice to Proceed and any amendments or reevaluation made during construction.
- Construction mitigation and proper disposal of Hazardous Materials in accordance with Regulations

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and Local Laws.

- Maintenance of access to abutters and local businesses throughout construction.
- Construction noise mitigation.
- Construction dust mitigation.

Major Progress as of November 30, 2025

Major project elements:

2025

- Borings and test pits conducted. Protective netting on existing bridge installed. Advance signage installed
- Erosion control measures installed. Clearing within entrance/exit ramps. Tree clearing along Route 9
- Utility poles installed by Eversource and wiring completed. Duct bank and manholes installed at South Abutment
- Gas line relocation in Route 9 EB and services completed by Eversource
- Retaining wall at 190 North Main Street ongoing with foundation and stone installation
- Coordination with abutters, Rt.27 Plaza, 190 North Main Street, Midas, ongoing.
- Public Involvement Coordination Meetings continuing.
- 1st and 2nd Structural Bridges (CE1 and CE2) Design submitted. 1st Structural Bridge (BC-7) Design submitted. 75% Highway Design submitted.
- ENR Article about the Project published in September 2025 edition

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Project Budget/Financials

The table below contains a summary of project financials through November 30, 2025:

Project Budget / Financials			
	<u>Contract</u>	<u>MassDOT</u>	<u>Municipal</u>
Bid	\$ 99,150,927	\$ 99,150,927	\$ 0
Allowances	\$ 3,916,000	\$ 3,916,000	\$ 0
Original Contract Value	\$ 103,066,927	\$ 103,066,927	\$ 0
Original Contingency (FIN681)		\$ 9,918,843	
Original Encumbrance		\$ 112,985,770	
Encumbrance Modifications		\$ 0	
Current Encumbrance		\$ 112,985,770	
Current Overruns and Underruns	\$ 0	\$ 0	\$ 0
Approved Contract Modifications	\$ 0	\$ 0	\$ 0
Current Contract Value	\$ 103,066,927	\$ 103,066,927	\$ 0
Pending Contract Modifications	\$ 60,823	\$ 60,823	\$ 0
Estimated Contract Total	\$ 103,127,750	\$ 103,127,750	\$ 0
Probable Contract Modifications		\$ 0	
Forecast Cost-at-Completion	\$ 103,127,750	\$ 103,127,750	\$ 0
Total CQE (Invoiced) to Date	\$ 17,743,926	\$ 17,743,926	\$ 0
Funds Remaining		\$ 95,241,844	
Projected Funds Remaining		\$ 9,858,019	
Expected Progress (baseline)	0.00%		
% Complete	17.21%		

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Schedule / Project Milestones

Milestones	Original Contract	Current Contract	Contractor Forecast*	Variance from Current Contract	District/OR Forecast
Contract Award	22-08-2024 (A)				
Notice to Proceed	19-09-2024 (A)				
MS #03 Full Beneficial Use	03-06-2030	03-06-2030	23-05-2030	11	03-06-2030
MS #02 Substantial Completion	24-06-2030	24-06-2030	10-06-2030	14	24-06-2030
MS #01 Final Completion	28-08-2030	24-06-2030	14-08-2030	14	28-08-2030

*Based on McCourt's Schedule Update No. 11 October 31, 2025.

Areas of Concern

The following represents the primary areas of concern identified in design and at this stage in construction:

- Removal of the existing bridge over Route 9
- The roadway and bridge construction staging will require careful consideration given the traffic volumes and lanes restrictions. The Design-Builder will be responsible for providing early release design packages to address staging and presenting the intended methods of traffic control to maintain traffic throughout construction.
- Impacts to abutters due to construction, specifically traffic/access and noise impacts. Coordination with abutters is ongoing. Some discussions have led to extra work orders.
- There are currently 9 extra work orders in review. The current value of these is \$572,630, with 3 of the extra work orders without associated values submitted.
- EWO-007 36-inch Trunkline and EWO-008 Stormwater Management Report are Notices of Delay associated with ERC-5 36-inch Trunkline. The Contractor is claiming delays that have delayed the start of ERC-5 construction and other associated submittals.

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Project Photographs



Protective Netting Installed on Existing Rt. 27 Bridge

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Utility Pole Installation by Eversource



Clearing at Rt. 9 EB/Rt. 27 SB Entrance/Exit Ramps.

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Duct Bank Manholes Installed at South Abutment



Wiring at New Utility Poles

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Retaining Wall Installation at 190 North Main Street

Project Name: Natick Bridge Replacement, N-03-020 Route 27 (North Main Street) over Route 9 (Worcester Street) and Interchange Improvements in Natick

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Gas Line Installation in Rt. 9 EB by Eversource

CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

During the construction phase of this project, Jacobs, as Engineer of Record, is continuing to provide construction phase services. In addition, this O.R. will continue to partially satisfy the peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and monitoring the change order process to ensure any proposed design changes are evaluated for their impacts to the project's quality, budget, or schedule goals.

Value Engineering

A Value Engineering (VE) Study of the project design concept was not performed.

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Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues.

Extra Work Orders

As of November 30, 2025, there are 9 extra work orders in review. The current value of these is \$572,630, with 3 of the extra work orders values not yet submitted. EWO-007 36-inch Trunkline and EWO-008 Stormwater Management Report are Notices of Delay currently without an associated value.

MassDOT Rt. 27 Bridge Extra Work Orders		11/30/2025	
EWO		Current Value	Status
EWO-001	Project Visual Plan Changes	\$141,669.12	In MassDOT D3 review
EWO-002	MSE Wall Special Provision	\$53,380.07	D3 Approved, In Boston review
EWO-003	75% Highway Design Changes	\$189,956.80	In MassDOT review
EWO-004	Midas Driveway Design	\$0.00	Removed, incorporated in to 75% design
EWO-005	Emergency Repairs T&M	\$7,443.35	Approved, paid
EWO-006	Midas Temp Access & ADA Ramp Construction	\$17,328.78	In MassDOT review
EWO-007	36-inch Trunkline (ERC-005)		Notice of Delay submitted
EWO-008	Stormwater Management Report		Notice of Delay submitted
EWO-009	190 N. Main Street Retaining Wall Changes	\$162,852.42	McCourt proceeding with construction, will be revised
EWO-010	9x27 Plaza Revisions		Contract Amendment letter submitted, scope to be revised
	Total	\$572,630.54	

OWNER'S REPRESENTATIVE OATH

I, Mark V. King, MA P.E. No. 47815, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120640, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



Mark V. King P.E.

Date: December 15, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Bowker Overpass Over I-90, CSX/MBTA and Ipswich Street

Owner's Representative Contract Number: 120637

Project Number: 606496

Construction Contract Number: 127762



OWNER'S REPRESENTATIVE'S 2025 ANNUAL REPORT

Owner's Representative Contract No.: 120637 Patrick Engineering, Inc. a RINA Company

Project No. 606496 BOSTON - BRIDGE SUPERSTRUCTURE REPLACEMENT AND WIDENING, B-16-052, BOWKER OVERPASS OVER I-90, MBTA/CSX AND IPSWICH STREET

Project Designer: Gill Engineering, Inc.

Contractor: Barletta Heavy Division, Inc.

Prepared by: Paul O. Mulroney, P.E.

December 15, 2025

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
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I. EXECUTIVE SUMMARY

The Bowker Overpass in Boston connects Boylston Street and the Fenway and Back Bay neighborhoods to Storrow Drive which services up to 130,000 vehicles per day and provides access to Interstate 93. The Bowker Overpass is classified as an urban arterial roadway. The overpass was constructed in 1964 and is a critical component of the city roadway infrastructure.

The existing Bowker Overpass over Interstate 90, MBTA/CSX Rail Lines and Ipswich Street (the Project) is structurally deficient. Inspections determined that the superstructure, substructure and deck were determined to be in poor condition with deficiencies that include (1) beams with substantial section loss and rust holes, (2) rusted or overexpanded bearings (3) spalled foundation column, walls and pier caps with exposed de-bonded reinforcing, (4) concrete deck exhibited cracks and delamination, (5) pot-holed and patched wearing surface, and (6) deteriorated parapets, curbs and sidewalks. The existing Bowker Overpass and associated roadway infrastructure also lacks the requisite infrastructure to support multimodal and ADA movements; and negatively impacts environmental and cultural resources in the area (i.e., the Muddy River, Charlesgate Park and Back Bay Fens).

The Project addresses the above issues through superstructure replacement and widening of Bowker Overpass over Interstate 90, MBTA/CSX Rail Lines and Ipswich Street with the addition of a new connection bridge at Charlesgate West between Boylston Street and Commonwealth Avenue. This new connection will allow the existing off ramp from the Bowker Overpass to Commonwealth Avenue to be removed, which will daylight the Muddy River at Charlesgate Park between Commonwealth Avenue and Newbury Street. A shared-use path for non-motorized/pedestrian traffic and dedicated bicycle lanes will be constructed adjacent to the new Charlesgate West connection bridge, and a new underpass connection to Charlesgate Park, park improvements, and significant landscaping on the shared-use path structure and bridge approaches will be provided.

The lead designer for the Project is Gill Engineering, Inc. The Contractor, Barletta Heavy Division, Inc. (BHD) was awarded Notice to Proceed (NTP) on April 1, 2025. Physical work on site began on August 7, 2025 with start of signage, erosion and sedimentation controls, temporary fencing and tree protections. As of November 30, 2025 the Project remains in the early phases of construction. The total Project encumbrance is \$103,632,120 and the contractual Contractor Field Completion Date is December 8, 2029.

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II. ANNUAL REPORT

BRIDGE SCOPE

The Project will include a full superstructure replacement and substructure rehabilitation of the Bowker Overpass, Bridge B-16-052(4FE), from the south abutment of Ipswich Street to Pier 4 at the north side Interstate 90 adjacent to Charlesgate Park. The existing bridge sits on concrete-filled steel pipe piles, driven to bedrock. These foundation elements will remain in place and the existing piers and other substructure elements will be rehabilitated. A replacement 4-span superstructure is to be placed on the rehabilitated pier walls, pier columns, pier caps and abutments. The superstructure consists of simple (non-continuous) spans and the deck employs link slabs for a jointless final condition.

A new bridge structure, Bridge B-16-052(CLH/CM6), will be constructed to carry Charlesgate West and the shared use path from Boylston Street to Commonwealth Avenue. The Charlesgate West bridge will consist of three spans and will include new piers, and modification of an existing retaining wall/abutment at Ipswich Street for use as the south abutment. An additional span to the north will be constructed for a pedestrian underpass to Charlesgate Park. The Charlesgate West bridge has an independent superstructure separate from the Bowker Overpass. Deep foundation drilled shafts into bedrock, footings and wall piers will support the bridge superstructure. Mechanically stabilized earth retaining walls will be employed for the new Charlesgate West approach roadway and shared use path.

The off-ramp bridge from the Bowker Overpass to Commonwealth Avenue over the Muddy River, Bridge B-16-365(4FD), will be removed which will open the Muddy River to daylight. The piers are proposed to be removed from 2' below the mudline within the Muddy River leaving the balance of the foundations in place. Bridge B-16-365(4EL), which is the section of the Bowker Overpass from above Interstate 90 Westbound to above Beacon Street, will be modified by removing the existing median and extending deck and roadway to facilitate traffic movements.

ROADWAY AND SHARED USE PATH SCOPE

The Boylston Street intersection will be realigned and reconfigured to accommodate the new Charlesgate West bridge ramp. The removal of the off-ramp to Charlesgate East from the Bowker Overpass and the addition of the two-way Charlesgate West bridge creates a four-legged intersection. Vehicles traveling westbound on Boylston Street will travel straight onto Charlesgate West and proceed to the intersection of Charlesgate West and Commonwealth

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Avenue. The revised alignment of Boylston Street eastbound removes the slip lane at the intersection at Bowker Overpass. The new intersection layout brings all travel lanes into the intersection and aligns the western approach of Boylston Street with the Bowker Overpass, and the eastern approach of Boylston Street with the Charlesgate West ramp.

Vehicles traveling eastbound on Boylston Street will no longer access Commonwealth Avenue from Charlesgate East as the existing ramp will be removed. The new intersection design provides for bi-directional travel on the Charlesgate West ramp for vehicles travelling to and from Commonwealth Avenue. Vehicles travelling eastbound on Boylston Street that need to access Commonwealth Avenue and local roads will now turn left onto Charlesgate West to access those roadways.

A shared-use path and separate dedicated bike lane will be constructed between the Bowker Overpass and the new Charlesgate West ramp. A landscaped buffer with trees and shrubs is included in the design between the shared use path and the dedicated bike lane. These design elements provide a fully accessible connection from Charlesgate Park and Commonwealth Avenue to Boylston Street and the Emerald Necklace to the south.

The proposed Charlesgate West bridge will also carry a 6' sidewalk on the west side in order to connect pedestrian traffic from Boylston Street to Newbury Street. A pedestrian underpass from Newbury Street into Charlesgate Park will also be constructed for better accessibility to the park.

CONSTRUCTION STAGING

Construction staging is a major aspect of the Project. A key constraint is the restriction on lane closures which prohibits full closure of Interstate 90 or the Bowker Overpass. Accordingly, various combinations of travel lane closures are anticipated to minimize traffic impacts.

The contract construction sequencing concept is generally as follows:

Stage 1A

- Repair the substructure of the Bowker Overpass Bridge B-16-052(4FE) at Ipswich Street South Abutment, Pier 2 between Interstate 90 and MBTA/CSX tracks, and Pier 4 north of Interstate 90 adjacent to Charlesgate Park.
- Construct Charlesgate West Bridge B-16-052(CLH, CM6) South Abutment at Ipswich Street, and the west portion of the Pier 3 and North Abutment foundations

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and shafts and footing substructures.

Stage 1B

- Repair of substructure of the Bowker Overpass Bridge B-16-52(FE) Pier 1 adjacent to the south MBTA/CSX tracks and Pier 3 in the median of Interstate 90.
- Construct the foundation and shaft and footing substructures of Charlesgate West Bridge B-16-052(CLH, CM6) Pier 1 adjacent to the south MBTA/CSX tracks and Pier 2 in the median of Interstate 90.

Stage 2A

- Demolish existing barriers on Bowker Overpass Bridge B-16-52(FE) and Bowker Overpass Bridge B-16-365, and form continuous deck with temporary barriers to accommodate staged construction.
- Erect steel girders and prestressed concrete beams on the west side of Charlesgate West Bridge B-16-052(CLH, CM6).
- Install conduits on Charlesgate West Bridge B-16-052(CLH, CM6) to accommodate electrical relocations.
- Cast concrete deck, bridge barriers and sidewalks for the west side of Charlesgate West Bridge B-16-052(CLH, CM6). Transfer pedestrian and on-ramp traffic from the existing Commonwealth Avenue Bowker Overpass on-ramp to the west portion of the new Charlesgate West Bridge B-16-052(FE).
- Relocate electrical lines to new conduits on Charlesgate West Bridge B-16-052(CLH, CM6).

Stage 2B

- Demolish on-ramp portion of Bowker Overpass Bridge B-16-52(FE) and existing Bowker Overpass Charlesgate West on-ramp from Commonwealth Avenue.
- Demolish Bowker Overpass Bridge B-16-52(FE) Span 3 sidewalk.
- Construct the east portion of foundation and shaft and footing substructure of Charlesgate West Bridge B-16-052(CLH, CM6) at Pier 3 and North Abutment.
- Erect steel girders and prestressed concrete beams on the east side of Charlesgate West Bridge B-16-052(CLH, CM6) and the shared use path.
- Cast concrete deck, bridge barriers, curbs and planters for the east portion of Charlesgate West Bridge B-16-052(CLH, CM6) and the shared use path.
- Pave, stripe and prepare Charlesgate West Bridge B-16-052(CLH, CM6) for vehicle and pedestrian traffic.

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Stage 3A

- Open Charlesgate West Bridge B-16-052(CLH, CM6) to bi-directional vehicle traffic and pedestrian use.
- Modify Bowker Overpass vehicular traffic to single lanes on the west side of the overpass structure.
- Close and demolish existing Bowker Overpass Bridge B-16-365 Charlesgate East off ramp to Commonwealth Avenue. Foundation to be demolished to elevations 2 feet below grade or mudline.
- Demolish the east superstructure of Bowker Overpass B-16-52(FE) and sidewalk on the east side of the vault structure south of Ipswich Street.
- Modify caps at east end of Piers 1-4.
- Erect steel girders on the east side of Bowker Overpass B-16-52(FE).
- Cast concrete deck and bridge barriers for the east portion of Bowker Overpass B-16-52(FE).
- Pave, stripe and prepare east portion of Bowker Overpass B-16-52(FE) for vehicle traffic.

Stage 3B

- Modify Bowker Overpass vehicular traffic to single crossover lanes on the east side of the overpass structure.
- Demolish the west superstructure of Bowker Overpass B-16-52(FE).
- Modify caps at west end of Piers 1-4.
- Erect steel girders on the west side of Bowker Overpass B-16-52(FE).
- Cast concrete deck and bridge barriers for the west portion of Bowker Overpass B-16-52(FE).
- Complete vault superstructure repairs.
- Final pavement, pavement markings, vehicle and pedestrian signals, opening.

MAJOR PROGRESS AS OF NOVEMBER 30, 2025

The Bowker Overpass Project delivery methodology is design-bid-build (DBB). The Project is currently in the early phases of construction. NTP was awarded to BHD on April 1, 2025, and physical work on site began on August 7, 2025 with start of signage, erosion and sedimentation controls, temporary fencing and tree protections. The following scope of work has been performed as of November 30, 2025:

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- Clearing, grubbing and tree removal
- Fencing
- Installation of temporary traffic controls
- Demolition of existing retaining wall at South Abutment (Ipswich Street)
- Drilling and grouting dowels at South Abutment
- Installation of rebar and placement of concrete for South Abutment backwall
- Installation of drilled shaft test shaft at South Approach
- Excavation, grading and installation of sheet pile support of excavation at North Approach (adjacent to Charlesgate West)
- Installation of soldier pile lagging and tie backs at Charlesgate West North Approach
- Excavation, sawcut and demolition of Newbury Street retaining wall
- Excavation, forming and installation of rebar and placement of concrete for Bowker Overpass Pier 4 footings
- Installation of rebar for Pier 4 walls
- Excavation, forming and installation of rebar for Bowker Overpass Pier 2 footings
- Installation of temporary poles and signals at Boylston Street Intersection
- Demolition of existing traffic islands and pave Boylston Street intersection

BUDGET

The Project is to be funded via 2024 Transportation Improvement Program for the Boston Metropolitan Planning Organization. The Project cost summary is as follows

• Bid Price	\$ 83,900,000
• Allowances	\$ 11,341,000
• Original Contract Value	\$ 95,241,000
• Contingency	\$ 8,391,120
• Encumbered Amount	\$103,632,120
• Current Overruns/Underruns	\$ 38,756
• Approved Contract Modifications	\$ 9,750
• Pending Contract Modifications	\$ 78,023
• Forecast Cost-at-Completion	\$ 95,367,530
• Amount Expended through November 30, 2025	\$ 2,707,749
• Budgeted Funds Remaining	\$100,927,371

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SCHEDULE/PROJECT MILESTONES

The latest Monthly Progress Schedule is Update No. 5 submitted on November 19, 2025 with at Data Date of October 31, 2025. The prior schedule submission (Update No. 4) was returned “Not Accepted – Recovery Schedule Required”.

Schedule Update No. 5 re-sequences activities to recover the impacts associated with differing site conditions encountered at the Charlesgate West North Approach Ramp during excavation and installation of support of excavation. Schedule Update No. 4 indicated that the direct impact of the differing site conditions on the work was approximately 3 weeks with the balance of the impact attributable to weather sensitive concrete work in the following two winter seasons which had an overall impact to Milestone #3 (Full Beneficial Use) of 316 calendar days. The Contractor’s Schedule Update No. 5 recovers all impacts and betters Milestones 1 and 2 by 49 calendar days and Milestone 3 by 17 calendar days respectively. A summary of the milestone dates as submitted in Schedule Update No.5 is set forth below.

Update No. 5 Milestone Event	Contract Date	Contractor's Forecast	Variance in Days
Milestone #3 - Full Beneficial Use	3/26/2029	3/9/2029	17
Milestone #2 - Substantial Completion	11/8/2029	9/20/2029	49
Milestone #1 - Contract Completion	12/8/2029	10/20/2029	49
Milestone #0 - Notice to Proceed	4/1/2025	4/1/2025	0

The baseline schedule critical path begins with the preconstruction activities and BHD’s mobilization to begin the foundation, pier and abutment work in Stages 1A and 1B. Once the foundations for the west half of the new Charlegate West Bridge are in place, steel erection and superstructure construction are progressed. The west portion of the completed Charlesgate West Bridge is then opened for single directional traffic on the Commonwealth Avenue on ramp.

Per the baseline schedule, Stage 2B follows with demolition of the existing Bowker Overpass Charlesgate West on-ramp, Pier 3 foundation work and North Abutment work for the east portion of the Charlesgate West Bridge followed by steel erection and superstructure work. Once the Charlesgate West Bridge is fully constructed and paved in Stage 2B, it is opened to bi-directional traffic from Boylston Street and Commonwealth Avenue.

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
Design Contract Number: 127762

Stages 3A and 3B constitute the sequenced construction of the existing Bowker Overpass superstructure replacement with the east half being completed in advance of the west half. The Contract closeout and punchlist follow the completion of the Bowker Overpass superstructure replacement and roadway work.

BHD's Schedule Update No. 5 revises the work performed in Stage 2A and Stage 2B by widening that portion of the Charlesgate West Bridge performed in Stage 2A by approximately 4 feet. The widening occurs immediately following the completion of the initial Charlesgate West Bridge section and, once widened it will accommodate bi-directional traffic to and from Boylston Street and Commonwealth Avenue at an earlier point in time that set forth in the contract phasing plan. Stage 2B of the Charlesgate West Bridge and the superstructure replacement of the Bowker Overpass will then be performed concurrently – recovering time on the schedule. The Contract Milestones and definitions are set forth below:

Milestone #03 (Full Beneficial Use) – 1,455 Calendar Days from Notice to Proceed (NTP)

Milestone #02 (Substantial Completion) – 1,682 Calendar Days from NTP

Milestone #01 (Contractor Field Completion) – 1,712 Calendar Days from NTP

Full Beneficial Use (MS#03) – The majority of the Contract Work has been completed and the asset(s) has been opened for full multi-modal transportation use, except for limited contract work items that do not materially impair or hinder public use of the transportation facility. All anticipated lane taking have been completed, except for minor, short term work items.

Substantial Completion (MS#02) – A walkthrough of the entire Contract Work has been performed by the Resident Engineer, a Punch List has been generated and the Work required by the Contract, including paperwork, has been completed, except for work have a Contract price of less than one percent of the adjusted Contract price, including overruns, underruns, and all Contract amendments. All material submittals have been received by the District Materials Lab.

Contractor Field Completion (MS#01) – All physical Contract Work is complete including punch list. The Contractor has fully de-mobilized from field operations.

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
Design Contract Number: 127762

RISKS AND POTENTIAL ISSUES

Several risks exist that could potentially affect cost and/or schedule. Each of the identified risks have been vetted by MassDOT and the design team. Primary concerns of this project and implemented mitigation devices are as follows:

- Working within or adjacent to the MBTA right-of-way could be impacted by lack of flaggers, delays in access approval or operational constraints. Commuter rail disruption due to work proximity and delays in submittal reviews by MBTA could negatively impact work. Allowances for engineering reviews, flagging resources and emergency diversion costs (e.g., busing) have been included in the budget. Communication channels have been developed and will be employed to facilitate construction.
- Construction damage to MBTA Green Line Tunnel from vibrations and/or changes in loading. Design mitigated risk by leaving existing pier footings in Muddy River 2 feet below mudline. Monitoring devices have been installed and will be observed during construction to detect any shifting or movement.
- Delays in relocating utilities, including electrical relocations on the Charlesgate West Bridge, may delay construction. Communication channels have been developed and will be employed to facilitate construction. MassDOT has developed a shared cost policy to incentivize utility company relocations.
- Fenway Park, hospital, colleges, impacted stakeholders and general public concerns regarding lane closures and detours may put the construction schedule at risk. Timely public outreach will be implemented throughout construction to educate the public to potential impacts while stressing the benefits of the Project.
- Scope creep associated with requests of third-party authorities, neighborhood associations and other stakeholders may stress the Project cost and/or schedule. Positive relationships have been established with stakeholders and will be used to further manage the construction process.
- Unforeseen differing site conditions, including geotechnical conditions or unknown utility conflicts could impact cost and schedule. Test pitting of potential conflict areas will be implemented for early identification.

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
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PROJECT PHOTOGRAPHS AND RENDERINGS

Project Rendering (looking south)



Project Rendering (looking north)



Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
Design Contract Number: 127762

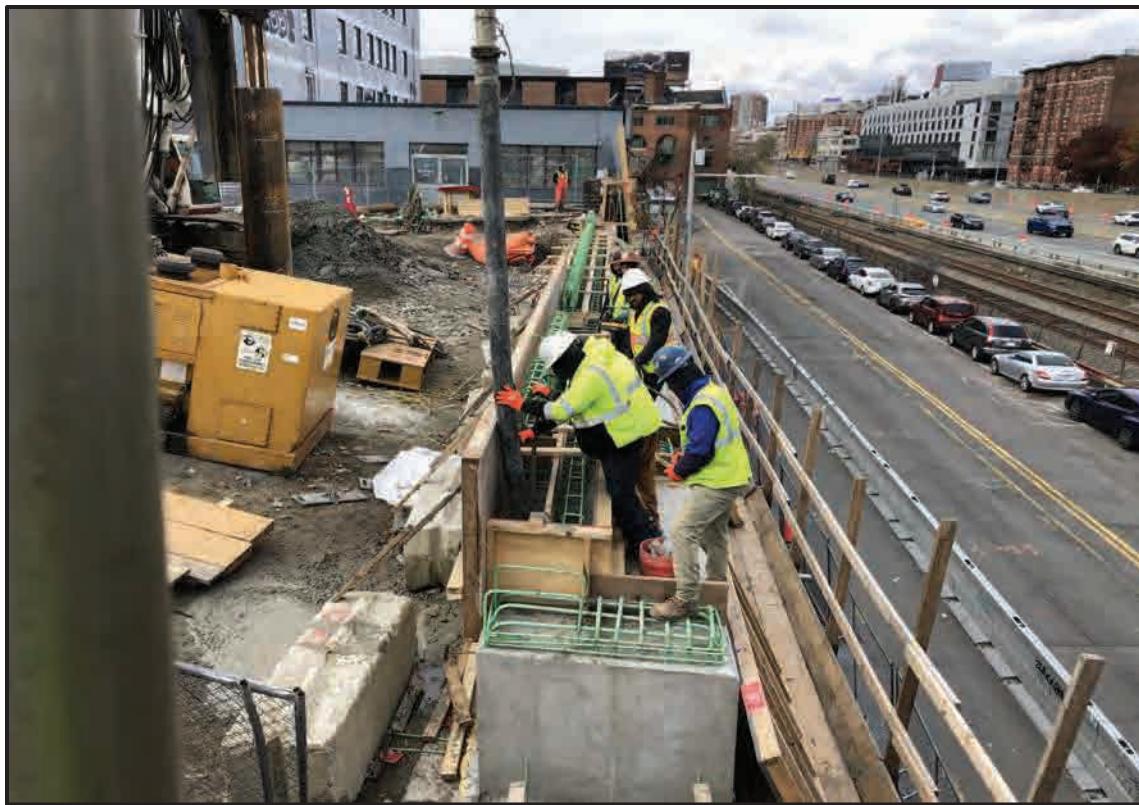


Tree Protection Fencing – September 15, 2025



Bowker Overpass Pier 4 Footing Excavation – October 10, 2025

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
Design Contract Number: 127762



Charlesgate West Bridge South Abutment Backwall Concrete Placement
November 12, 2025

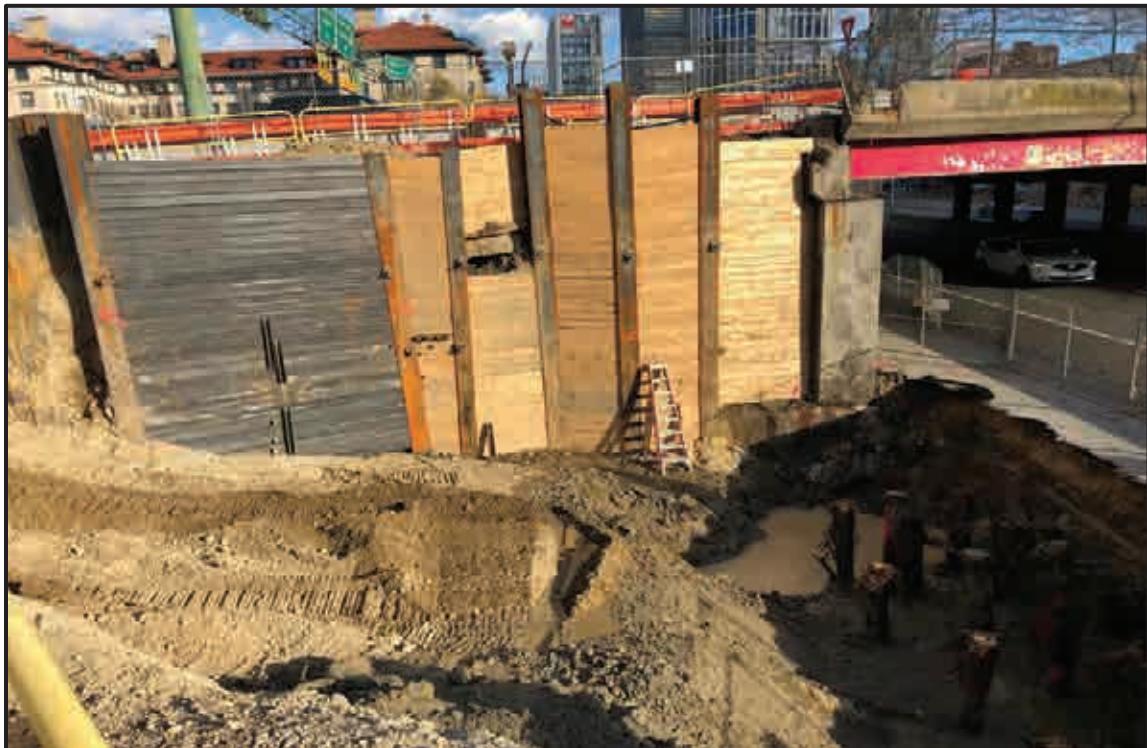


South Approach Test Shaft – November 12, 2025

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
Design Contract Number: 127762



Newbury Street Retaining Wall Rehabilitation – November 24, 2025



Charlesgate West North Support of Excavation and Timber Piles – November 24, 2025

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637
Project Number: 606496
Design Contract Number: 127762

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

PEER REVIEW

Project peer reviews were performed by MassDOT personnel, the City of Boston and project stakeholders as part of the review process for the 25%, 75% and 100% design submissions. Alfred Benesch & Company participated in the design reviews as an independent third party for structural bridge submissions. SMK Services, Inc. participated as a constructability reviewer.

During the construction phase of this project, this Owner's Representative will satisfy peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon any proposed design changes and their impacts to the project's quality, budget, or schedule goals.

VALUE ENGINEERING

In November 2023, MassDOT initiated a Value Engineering (VE) study to accomplish the project's goals in the most expeditious and cost-effective manner. The objective of the study was to identify opportunities and to recommend alternatives that improve value in terms of capital cost reductions, improved constructability and product enhancement while providing the basic functional requirements to meet the Project intent. HNTB conducted the VE study from November 13, 2023 through November 17, 2023.

COST RECOVERY

At this point in time, no issues have been specifically identified as Cost Recovery issues. The Owner's Representative will monitor issues throughout construction and will implement a formal cost recovery process. If potential cost recovery issues are identified, the Owner's Representative, with MassDOT Highway Division, will discuss referral to the Cost Recovery Standing Committee. For any issue referred for Cost Recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 8/7/17) shall govern. The Owner's Representative, as Primary Manager of

Project Name: Bowker Overpass Over I-90,
MBTA/CSX and Ipswich Street
Owner's Representative Contract Number: 120637

Project Number: 606496
Design Contract Number: 127762

Cost Recovery under legislative mandate, will follow the referenced SOP. This SOP, as modified for projects with an Owner's Representative, describes the role of the Owner's Representative and includes a Cost Recovery Standing Committee. This committee oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary.

OWNER'S REPRESENTATIVE OATH

I, Paul O. Mulroney, P.E., MA P.E. No. 44048, hereby certify that my sole responsibility as Owner's Representative, under OR Contract 120637, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

Paul O. Mulroney, P.E.
Owner's Representative

Date: December 15, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator

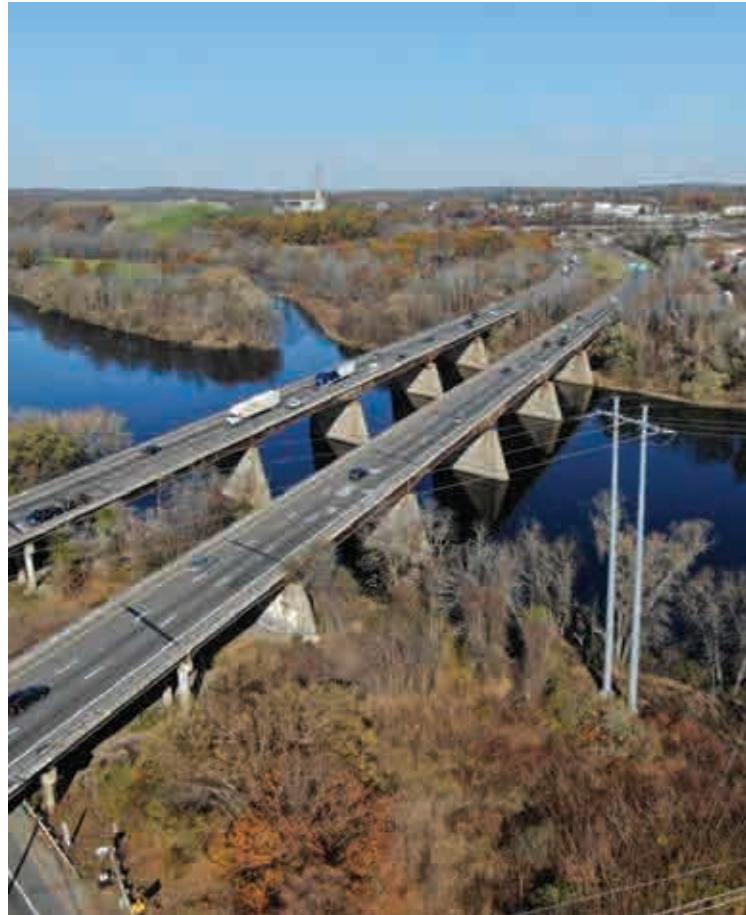


Project Name: Methuen Haverhill I-495 over Merrimack River

Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251



Owner's Representative's 2025 Annual Report

Owner's Representative Contract No.: 120636 Millennium Engineering Inc.

Project No. 609466: Methuen-Haverhill I-95 over the Merrimack River

Preliminary Designer: VHB

Design-Builder: SPS-DW White JV

Prepared by: Millennium Engineering, Inc.

December 16, 2025

Ten Park Plaza, Suite 4160, Boston, MA 02116

Tel: 857-368-4636, TTY: 857-368-0655

www.mass.gov/massdot

Project Name: Methuen-Haverhill I-495 over the Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 609466
Construction Contract Number: 126251

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Project Name: Methuen-Haverhill I-495 over the Merrimack River

Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251

I. EXECUTIVE SUMMARY

MassDOT proposes replacing the northbound and southbound bridges carrying I-495 northbound and southbound over the Merrimack River (Bridge No. H-12-0410=M-17-020) and over Routes 110/113 (Bridge No. M-17-031) between Methuen and Haverhill, Massachusetts.

As part of the interstate system, the bridges serve as a significant local and regional route, carrying close to 125,000 vehicles per day and over 9,000 vehicles per hour during peak periods. As these bridges approach 60 years of use, the structural condition is listed as poor. As such, the Project's purpose is to replace these bridges with structures that will provide a minimum 75-year design life and meet current criteria for lane and shoulder widths. Over both bridges, the existing cross section consists of a four-foot inside (left) shoulder, three 12- foot travel lanes, and a six-foot outside (right) shoulder. As part of the Project, the inside (left) and outside (right) shoulders will be widened. With the Project in place, on the bridges there will be three 12-foot travel lanes in both directions, with 10-foot minimum outside (right) shoulders, 18-foot minimum inside (left) shoulders, and two feet of buffer to the bridge rail on each side. Off the bridges, there will be three 12-foot travel lanes in both directions with 10-foot minimum outside (right) shoulders and a four-foot minimum inside (left) shoulder. The Project is proposed to maintain three travel lanes in each direction. The bridges are located between Exit 105 (Route 213) and Exit 106 (Industrial Avenue). The Project is not anticipated to permanently impact the geometric configuration or traffic operations of the upstream and downstream ramps at Exit 105 (Route 213) and Exit 106 (Industrial Avenue).

MassDOT went through the Design/Build procurement process in 2024, with the best value bidder being the SPS-DW White JV/HNTB team in the amount of \$337,584,363.00. The Middlesex Corp filed a bid protest that was rejected. However, the appeal delayed the Contract Award until March 5, 2025 and the Notice to Proceed until March 20, 2025. Based on these delays, the design-build team has submitted numerous schedules showing that the project completion will be delayed. MassDOT has rejected those schedules and is working with the team to recover the lost time. As it stands now, MassDOT and SPS-DW White will have numerous meetings throughout early 2026 to work through the details of how the time will be recovered. Also, it is too early to know what if any fiscal impacts may result from this issue.

Project Name: Methuen-Haverhill I-495 over the Merrimack River

Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251

II. ANNUAL REPORT

Contract Scope of Work:

The Methuen-Haverhill Bridge Replacement project consists of replacing both the Northbound and Southbound I-495 Bridges over the Merrimack River and Routes 110/113. The existing bridges were constructed in 1964 and consist of a cast-in-place concrete deck on steel girders. Each bridge consists of 10 spans, five of which are simply supported and a 5 span continuous portion over the river. The southern abutments are supported by cast-in-place concrete piles, while the northern abutments are supported by steel h-piles and spread footings. The Piers are founded upon H-piles, cast-in place concrete piles or spread footings. Piers consist of solid tapered wall cast-in-place concrete with granite facing on the lower portions subject to river flow. Abutments are cast-in-place concrete with battered large stone rip-rap protection on steeply battered slopes. The overall bridge length of the northbound bridge is 1115'-7" while the southbound total bridge length is 1106'-7". The proposed structure will increase the bridge width from the existing 52'-2" to 79'-6 1/2" and 71'-6 1/2" on the northbound and southbound barrels respectively. The roadway vertical profile will be slightly raised, and the proposed bridge will provide a minimum 70'-0" wide by 30'-0" high marine navigation corridor in the center of span 2. Proposed spans are 217'-0", 278'-0" and 217'-0".

Major Progress as of November 30, 2025

During 2025, The SPS-DW White team made numerous design submittals in support of the proposed construction activities. In addition, the following construction work has taken place:

- Construction of the temporary trestle in the Merrimack River
- Clearing in the median south of the river for the Highway realignment.
- Emergency deck repairs on the I-495 NB and SB bridges and the Industrial Ave. bridge.
- Emergency steel repairs and temporary support of the I-495 NB &SB bridges
- Begin installation of the cofferdams for stage 1
- Stage 1 traffic shifts on I-495 SB
- Deployment of the Real Time Traffic Monitoring System

Project Name: Methuen-Haverhill I-495 over the Merrimack River

Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251

Budget:

- Office Estimate: \$351,800,000.00
- Bid Price: \$337,584,363.00
- Encumbered* Amount: \$381,349,694.30
 - *Includes Contingency, Police, Phone, and Trainees
- Total Amount Expended through November 30, 2025: \$50,140,296.00
- Current Estimate at Completion: \$348,323,075

Project Budget / Financials			
	<u>Contract</u>	<u>MassDOT</u>	<u>Municipal</u>
Bid	\$ 337,584,363	\$ 337,584,363	\$ 0
Allowances		\$ 10,004,000	\$ 0
Original Contract Value	\$ 347,588,363	\$ 347,588,363	\$ 0
Original Contingency (FIN681)		\$ 33,761,331	
Original Encumbrance		\$ 381,349,694	
Encumbrance Modifications		\$ 0	
Current Encumbrance		\$ 381,349,694	
Current Overruns and Underruns	\$ 637,500	\$ 637,500	\$ 0
Approved Contract Modifications	\$ 60,000	\$ 60,000	\$ 0
Current Contract Value	\$ 348,285,863	\$ 348,285,863	\$ 0
Pending Contract Modifications		\$ 37,212	\$ 0
Estimated Contract Total	\$ 348,323,075	\$ 348,323,075	\$ 0
Probable Contract Modifications		\$ 0	
Forecast Cost-at-Completion	\$ 348,323,075	\$ 348,323,075	\$ 0
Total CQE (Invoiced) to Date	\$ 50,140,296	\$ 50,140,296	\$ 0
Funds Remaining		\$ 331,209,399	
Projected Funds Remaining		\$ 33,026,619	
Expected Progress (baseline)	0.00%		
% Complete	14.39%		

Schedule:

- Advertisement Date: June 13, 2024
- Bid Opening Date: October 24, 2024
- Award Date: March 5, 2025
- Notice to Proceed (NTP) Date: March 20, 2025
- Substantial Completion and Full Beneficial Use: August 23, 2030
- Construction Contract Completion/Final Acceptance: October 14, 2030

Project Name: Methuen-Haverhill I-495 over the Merrimack River

Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251

Areas and Issues of Concern

- Due to the bid protest filed by The Middlesex Corporation the contract award and Notice to Proceed were delayed. This has caused the SPS-DW White team to submit schedules showing that the project completion will also be delayed. MassDOT has rejected all of these submittals and is working with the design builder to mitigate the potential impacts associated with this delay. It is believed that the schedule will be recovered and the project completed on time, however, there may be cost implications.
- The design accepted by MassDOT during the selection process requires that several of the environmental permits be resubmitted for approval due to the differing environmental impacts associated with the changes. However, most of the impacts are less than the Base Technical Concept impacts so it is expected that approval will be received avoiding any delays.

Progress Photographs



Borings in River

Project Name: Methuen-Haverhill I-495 over the Merrimack River

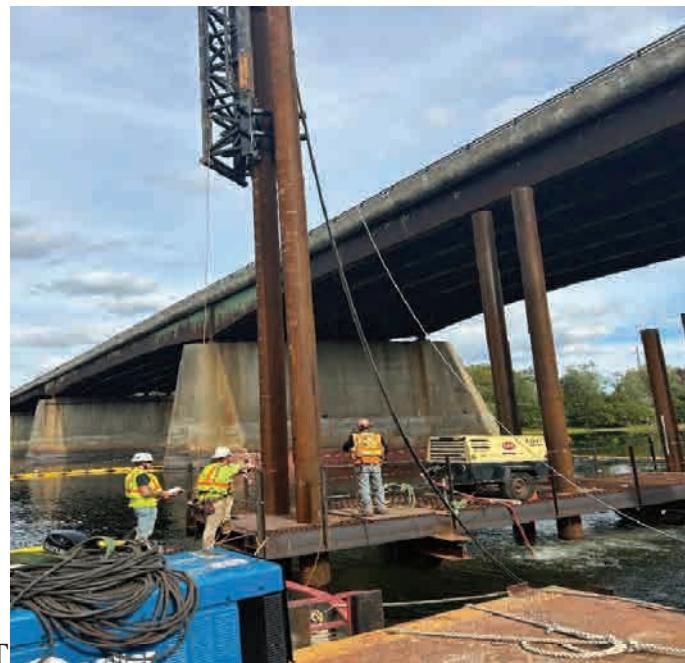
Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251



Emergency Deck Repairs



Trestle Work

Project Name: Methuen-Haverhill I-495 over the Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 609466
Construction Contract Number: 126251

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

During the design phase of this project, a peer review was performed at each stage of the design by MassDOT's Highway Division forces independent of the project designer. These reviews were performed to identify conflicts or errors that should be corrected or mitigated at each stage before design progressed further. This Owner's Representative was not involved during the preliminary design phase but has determined that the peer review was performed through discussions with MassDOT personnel.

Value Engineering

There was no specific Value Engineering study performed; however, the Design-Build procurement method is considered a Value Engineering technique.

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues. However, it should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for its referral to the Cost Recovery Standing Committee. For any issue referred for Cost recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 9/25/06) shall govern. To fulfil the legislative mandate of being the Primary manager of cost recovery, the Owner's representative will follow the referenced SOP. This SOP, as modified for projects with an owner's representative, describes the role of the Owner's representative and includes a Cost Recovery Standing Committee, which oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary. It should also be noted that the resolution of cost recovery issues typically does not start until the end of a project, to ensure working relationships are maintained and insurance notifications occur only once a final decision has been made. In addition, in moving forward with future resolution of Cost recovery issues, it is recommended that the resolution follow the path set forth in the recently developed Draft MassDOT Owner's Representative Standard Operating Procedures.

Project Name: Methuen-Haverhill I-495 over the Merrimack River

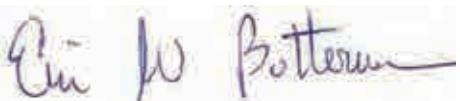
Owner's Representative Contract Number: 120636

Project Number: 609466

Construction Contract Number: 126251

Oath

I, Eric W. Botterman, P.E., MA P.E. No. 41099, hereby certify that my sole responsibility as Owner's Representative, under Owner's Representative Contract 120636, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.

A handwritten signature in blue ink that reads "Eric W. Botterman".

Eric W. Botterman, P.E.

Date: December 16, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



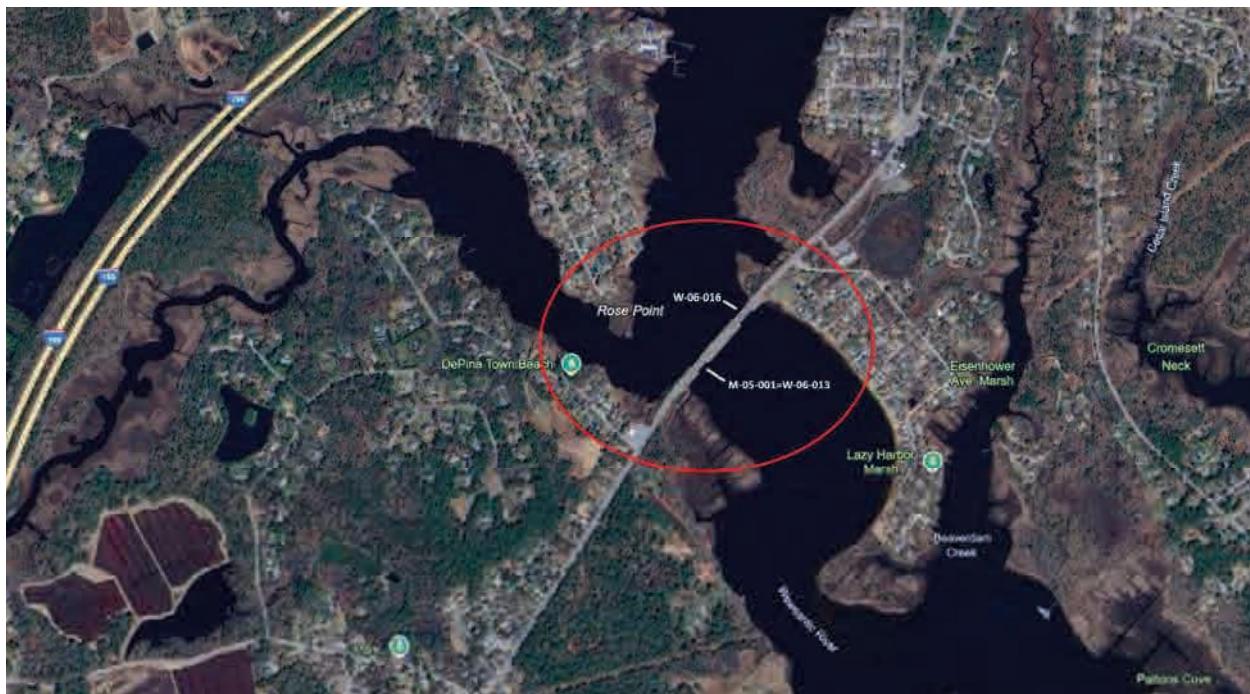
Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,

Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative Contract No. 129894 – Andrew S. Bradshaw, Jacobs Engineering Group, Inc.

Owner's Representative Work Order No. 1

Contract No. 128035 – Bridge Replacement, M-05-001=W-06-013 & W-06-016, Marion Road/Wareham Road (Route 6) over Weweantic River

Design-Builder: Northern Construction Service LLC & Benesch

Preliminary Designer: Parsons

Prepared by: Andrew S. Bradshaw, PE

December 11, 2025

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

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Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

I. EXECUTIVE SUMMARY

The two bridges over the Weweantic River connecting the towns of Wareham and Marion along Route 6 are structurally deficient and in need of replacement. These two bridges were built in the 1920s, widened in the 1950s, and are functionally obsolete.

MassDOT contracted Parsons to prepare preliminary design documents to be used as the Base Technical Concept (BTC) for a Best Value Design-Build Procurement process to accelerate delivery of the project. The project was awarded to Northern Construction Service, LLC with lead designer Benesch in March of 2025.

The purpose of the project is to improve public safety and improve mobility for all users throughout the project. Project goals include addressing structural deficiencies by replacing two functionally obsolete bridges with new code-compliant bridges which, thanks to their precast concrete superstructure, should also be easier to maintain, designing bridges to withstand coastal storm conditions and loadings, and improving roadway and sidewalk facilities to comply with current standards. The design life is set at 40 years for roadway pavements and 75 years for bridges.

The work under this project includes the design and construction of the removal and replacement of both the superstructure and substructure for both bridges as well as the associated roadway, utility, retaining wall, and grading work. The project includes wide sidewalks which function as a shared-use-path. As the roadway is an important connection for the residents of these communities, it will be completed in a phased manner that allows traffic to continue to use the bridges in both directions throughout construction.

MassDOT and Northern Construction have committed to keeping the public informed and managing impacts on abutters and the traveling public as much as practicable. A public information meeting was held on August 21, 2025.

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

This project is in an environmentally and hydraulically sensitive area requiring coastal engineering, scour analysis, maintaining navigation channels, turtle protection, tidal wetland mitigation, and salt marsh replication among other environmental and hydraulic concerns.

As of November 30, 2025, the project is primarily in design and permitting. Construction activities have included enabling work such as tree clearing and utility relocation. The Project contains a Contractual Notice to Proceed date of March 26, 2025 and a Contractual Completion date of September 10, 2028, resulting in a contract duration of approximately 42 months or 3½ years. The total project encumbrance and original bid amount is \$68,897,200.

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

II. ANNUAL REPORT

Project Scope of Work

The project is in the towns of Marion and Wareham in southeastern Massachusetts. The Massachusetts Department of Transportation, Highway Division (MassDOT) proposes complete replacement of both M-05-001=W-06-013 and W-06-016 which carry Marion Road/Wareham Road (Route 6) over the Weweantic River. The existing M-05-001=W-06-013 bridge was two-spans, the proposed BTC replacement was two-spans totaling 136', and the proposed replacement utilizing the ATC process is a single span bridge with a span of 123'-6". The existing W-06-016 bridge was three-spans, the proposed BTC replacement was three-spans totaling 184', and the proposed replacement utilizing the ATC process is a two-span bridge totaling 173'-6". The replacement bridges are longer, wider, and higher than the existing structures to accommodate roadway improvements and coastal design/resiliency.

The existing bridges were built in the 1920s, widened in the 1950s, and are functionally obsolete thus requiring full replacement. To facilitate the roadway widening and larger 10' wide sidewalks/shared use paths, the project requires retaining walls along the edge of the roadway. Roadway work extends nearly one-half mile from the project limits on one side of the bridges to the project limits on the other side of the bridges and ranges from full depth replacement to raising the roadway several feet with the use of lightweight aggregate. Drainage and utility work extend throughout the project site.

Environmental permitting, turtle protection plans, salt marsh replication, and tidal wetland mitigation are among the work requirements for this tidal and environmentally sensitive area.

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

Major Progress as of November 30, 2025

The first eight months of the project have seen a lot of progress in design as shown by the timeline of major submittals/events that follow:

- Notice to Proceed: March 26, 2025
- Environmental Boring Plan Accepted: May 2025
- Project Management Plan Accepted: July 2025
- Quality Management Plan Accepted: July 2025
- Geotechnical Boring Plan Accepted: July 2025
- Bridge Demolition Plans Accepted: August 2025
- Bridge Sketch Plans Accepted: August 2025
- Baseline Schedule Accepted: September 2025
- 75% Highway Submission Accepted: November 2025
- 1st Bridge Submission W-06-016 Accepted: November 2025
- 1st Bridge Submission M-05-001=W-06-013 Accepted: November 2025

Budget

The project cost summary is as follows:

- Office Estimate: \$54,248,344
- Bid Price: \$68,987,200
- Encumbered Amount (MassDOT): \$68,987,200
- Forecast Cost-at-completion: \$68,987,200
- Anticipated Budget (Over)/Underrun-at-completion: \$ 0
- Total Amount Expended through November 30, 2025: \$ 15,238,375

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

Schedule/Project Milestones

The Project contains a Contractual Notice to Proceed date of March 26, 2025 and a Contractual Completion date of September 10, 2028, resulting in a contract duration of approximately 42 months or 3½ years. The only other project milestone is the Full Beneficial Use date of June 16, 2028.

Project Design is scheduled to be completed by December 13, 2025 per the accepted schedule. Construction is scheduled to be completed by May 25, 2028 per the accepted schedule.

Areas of Concern

The primary area of concern is the increased time and difficulty associated with many of the environmental permits and things such as the turtle protection plan. The delay in these permits/plans being accepted may delay construction and the overall project schedule.

A secondary area of concern is design completion is behind schedule and will not be completed/accepted by December 13, 2025, as originally planned. Currently, the delay appears to be covered by float in the schedule, but increased delays may result in a delay to the overall project schedule.

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

Project Photographs



Wareham Approach (both bridges)
looking towards Marion



Marion Approach (both bridges)
looking towards Wareham



Bridge M-05-001=W-06-013
looking towards Marion

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035



Bridge M-05-001=W-06-013
North elevation
looking towards Wareham



Bridge W-06-016
North elevation
looking towards Marion



Typical bridge underside

Project Name: Bridge Replacement, M-05-001=W-06-013 & W-06-016,
Marion Road/Wareham Road (Route 6) over Weweantic River

Owners Representative Contract Number: 129894

Project Number: 605311

Construction Contract Number: 128035

III. Chapter 303 of the Acts of 2008 Areas of Emphasis

Peer Review

A Peer Review has not been conducted for this project. During the construction phase of this project, this O.R. will satisfy peer review obligations by attending regular project review meetings where construction issues, conflicts, and resolutions are discussed; walking the site to observe major construction activities; and participating in the change order process, to review and comment upon any proposed design changes and their impacts on the project's quality, budget, or schedule.

Value Engineering

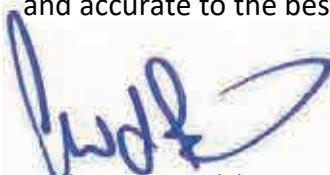
A formal Value Engineering (VE) study has not been conducted for this project, however, MassDOT views the confidential ATC Process as a type of value engineering.

Cost Recovery

As any significant construction has yet to commence, no Cost Recovery issues have been identified to date.

Oath

I, Andrew S. Bradshaw, PE, (MA No. 45173), hereby certify that my sole responsibility as Owner's Representative, under OR Contract 129894, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.



Andrew S. Bradshaw, PE | Owner's Representative | December 12, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Philip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation

Owner's Representative Contract Number: 120633

Project Number: 606527

Construction Contract Number: 127763



OWNER'S REPRESENTATIVE 2025 ANNUAL REPORT

Owner's Representative Contract No. 120633 – Greenman-Pedersen, Inc.

Project No. 606527 New Bedford – Route I-195 to Route 18 Interchange Rehabilitation

NEW BEDFORD, MASSACHUSETTS

Preliminary Design Consultant: CDR Maguire, Inc.

Design-Builder: D.W. White, Inc. – SPS New England, Inc. Joint Venture

Engineer of Record: HNTB, Inc.

Prepared by: Greenman-Pedersen, Inc.

December 15, 2025

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation

Owner's Representative Contract Number: 120633

Project Number: 606527

Construction Contract Number: 127763

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I. EXECUTIVE SUMMARY

This project includes the replacement of the existing 1,350-foot-long viaduct bridge structure, which carries Route I-195 EB and WB over the Route 18 interchange, Mass Coastal and Keolis Commuter Railroad, County Street, Purchase Street, State Street, and Weld Street in the City of New Bedford. The existing 15-span viaduct will be replaced with a proposed 7-span structure. The project also includes improvements to 3 other bridges along I-195, specifically the North Front Street and Bellville Avenue underpasses in New Bedford and the bridge over the Acushnet River in Fairhaven. The project also includes stormwater improvements, utility relocations, a new shared-use path and park, and reconstruction of the skate park under the viaduct. A location map of the general project area and specific project limits are shown in Figures 1 and 2, respectively. This project is being constructed by the Massachusetts Department of Transportation - Highway Division with CDR Maguire, Inc. serving as the Preliminary Design Consultant.

This project was advertised for Best Value Design-Build Procurement in March of 2024. A Notice to Proceed was issued to the Joint Venture of D.W. White – SPS New England, in association with HNTB, Inc. on April 7, 2025.

The following is a summary of the major progress since the NTP:

- The existing median along I-195 was demolished, deck repairs identified and performed, drainage installed, and the area graded and paved to support a traffic crossover. In late November, traffic was shifted to move the two lanes of traffic in each direction along I-195 onto the south (EB) side of the viaduct, allowing for demolition and replacement of the north side. This traffic shift is anticipated to be in place through the summer of 2027 as demolition and construction of the northern (WB) section of the bridge progresses.
- Ramp C (I-195 WB to Route 18 SB) and Ramp D (Route 18 NB to I-195 WB) were closed on 09/08/2025 with detours implemented. Ramp C has been demolished. These ramps will remain closed at least through the summer of 2027.
- Various utility and stormwater relocations and upgrades are ongoing within City streets.
- The project design advanced with the 75% Design Package and Final Bridge Sketch Plans being submitted and approved in September.

The project is approximately 20% spent, which is consistent with the cost-loaded project schedule. There are no Extra Work Orders that have been submitted to date. There is potential for Extra Work Orders in the future based on required bridge deck repairs in the viaduct median and contaminated soil discovered during construction.

The project is on schedule with Full Beneficial Use anticipated to be achieved on September 6, 2030.



Figure 1 – General Project Area

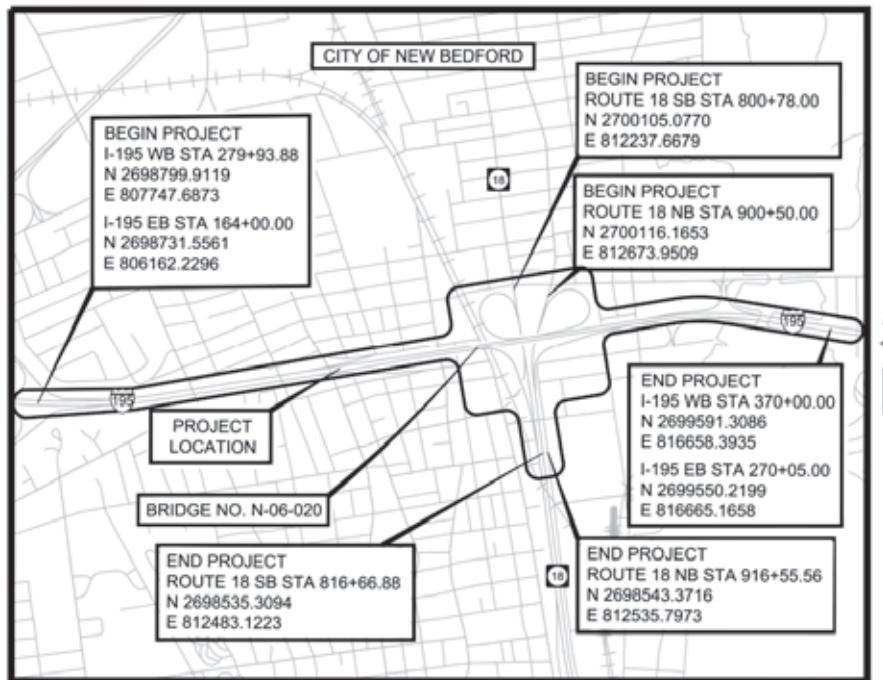


Figure 2 – Specific Project Limits

II. ANNUAL REPORT

Contract Scope of Work

This is a Design-Build project that includes the following key features:

Replacement of the structurally deficient viaduct bridge, N-06-020 (“the viaduct”), carrying I-195 (EB & WB) and Ramps C & F over Route 18, County Street, State Street, Mass Coastal and Keolis Commuter Railroad, Purchase Street, and Weld Street. The work includes widening the structure to improve safety for merging traffic to and from Route 18, while improving aesthetics with more open spans.

Improvements to bridge N-06-021 and N-06-022 which carry Route I-195 (EB & WB) over North Front Street and Belleville Avenue, respectively, both in the City of New Bedford and F-01-008 which carries Route I-195 (EB & WB) over the Acushnet River in the Town of Fairhaven.

New Stormwater Drainage system disconnecting highway drainage from the City's Combined Sewer Overflow (CSO) system.

Construction of a noise barrier along I-195 WB.

Replacement of highway lighting and signage throughout the project limits.

Construction of a Shared-Use Path for pedestrians, providing connections to Coggeshall Street and Weld Street from Purchase Street, and placemaking through the addition of a pocket park in the highway infield area.

Reconstruction of parking lots for the adjacent Hayden McFadden Elementary School.

Reconstruction of a skate park under the viaduct, including an accessible plaza.

Major Progress as of November 30, 2025

This project was advertised for Best Value Design-Build Procurement in March of 2024. Three Design-Build teams were prequalified through the procurement process, and the Joint Venture of D.W. White – SPS New England (DWW-SPS JV), in association with HNTB, Inc., was

determined to provide the best value to MassDOT. A Notice to Proceed (NTP) was issued to the DWW-SPS JV on April 7, 2025. The following is a summary of the major progress made by DWW-SPS JV since the NTP:

- Weekly Project Coordination and Weekly Utility Coordination meetings were initiated in April 2025 and are ongoing.
- Various Early Release for Construction (ERC) and Early Release for Fabrication (ERF) design package were submitted and accepted this year, including packages for the Drainage Trunkline ERC, Phase 1 and 2A Demolition Civil and Structural Components ERCs, Real Time Traffic Monitoring (RTTM) ERC, and I-195 WB and Ramp C (I-195 WB to Route 18 SB) Steel Joints and Bearings ERF. These packages were combined with various Over the Shoulder (OTS) meetings and Comment Resolution Meetings (CRMs) with the Department.
- The 75% Design Package was submitted on 09/19/2025 and final Bridge Sketch Plans were submitted on 09/24/2025. Both packages were accepted in conjunction with various OTS and CRM meetings with the Department.
- A Public Information Meeting was held on 06/26/2025 as required by Contract. Several other meetings have been held with legislatures, New Bedford City Council and other City officials, special interest groups and the public.
- The RTTM system and 360 cameras were installed.
- Security fencing was installed around the I-195/Route 18 infields to secure the primary work area.
- Erosion Controls were installed and clearing and grubbing was completed throughout the project limits.
- An additional parking lot for the Hayden McFadden School was constructed along Purchase Street, and the parking areas adjacent to Cedar Grove Street below the viaduct were reconstructed after utility installations in this area. These parking lots were fully functional for school staff and visitors prior to the start of the 2025-2026 school year.

- Ramp C (I-195 WB to Route 18 SB) and Ramp D (Route 18 NB to I-195 WB) were closed on 09/08/2025 with detours implemented through October 2027. Ramp C has been demolished. These ramps will remain closed at least through the summer of 2027.
- The I-195 median barrier and guardrail were demolished, drainage was installed, and the median was graded and paved to accommodate a traffic crossover from I-195 WB to the EB side. Temporary lighting was installed along I-195. In late November, traffic was shifted to move the two lanes of traffic in each direction along I-195 onto the south side of the viaduct, allowing for demolition and replacement of the north side of the viaduct. This traffic shift is anticipated to be in place through the summer of 2027.
- The need for deck repairs within the median of the viaduct was identified by the DWW-SPS JV upon removal of the median barrier. HNTB performed inspections of the deck after concrete median curb removal and developed a repair plan, proposing full depth deck repairs at various locations within the median area of the viaduct. These repairs represent approximately 5 to 10% of the exposed median deck area, mostly located adjacent to the bridge joints. This repair work was completed in October. Contract unit prices included for contingency purposes are expected to be utilized for this work.
- Various utility and stormwater relocations and upgrades were constructed and are ongoing within City streets, both by the DWW-SPS JV and the utility owners.

Budget

The following is a summary of the project budget. This project has no Municipal Encumbrance.

Office Estimate:	\$315,518,195
Bid Price:	\$295,225,000
Original Encumbered Amount:	\$336,864,371
Current Encumbered Amount:	\$336,864,371
Total Amount Expended through November 30, 2025:	\$61,058,829
Current Estimate at Completion:	\$307,340,060

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation

Owner's Representative Contract Number: 120633

Project Number: 606527

Construction Contract Number: 127763

The table below contains a summary of project financials through November 30, 2025:

Project Budget / Financials		
	Contract	MassDOT
Bid:	\$ 285,225,000	\$ 285,225,000
Allowances:	\$ 12,115,600	\$ 12,115,600
Original Contract Value:	\$ 307,340,600	\$ 307,340,600
Original Contingency (FIN81)		\$ 29,523,771
Original Encumbrance:		\$ 336,864,371
Encumbrance Modifications:		\$ 0
Current Encumbrance:		\$ 336,864,371
Current Overruns and Underruns:	\$ 0	\$ 0
Approved Contract Modifications:	\$ 0	\$ 0
Current Contract Value:	\$ 307,340,600	\$ 307,340,600
Pending Contract Modifications:	\$ 0	\$ 0
Estimated Contract Total:	\$ 307,340,600	\$ 307,340,600
Probable Contract Modifications:		\$ 0
Forecast Cost-at-Completion:	\$ 307,340,600	\$ 307,340,600
Total CCF (Invoiced) to Date:	\$ 61,058,829	\$ 61,058,829
Funds Remaining:		\$ 275,805,542
Projected Funds Remaining:		\$ 29,523,771
Expected Progress (baseline):	0.00%	
% Complete:	19.87%	

Schedule / Project Milestones

The following dates represent the DWW-SPS JV's latest Schedule Update 06, dated 11/17/ 2025:

- Notice to Proceed: April 7, 2025
- Full Beneficial Use: September 6, 2030 (1 day early)
- Substantial Completion: September 27, 2030 (51 days early)
- Final Completion: November 12, 2030 (49 days early)

These dates are identical to the dates included in the DWW-SPS JV's baseline schedule, accepted on 07/28/2025.

Areas of Concern

The following is a summary of items that are currently being discussed and worked through that may represent extra costs or delays to the project:

- The DWW-SPS JV has performed bridge deck repairs on the I-195 viaduct median as previously described herein. The cost implications of this work should be minor and are anticipated to be paid for utilizing contract pay items, which were included in the contract for contingency purposes.
- Modifications to the horizontal and vertical locations of the proposed noise wall along Ramp A (Cedar Grove Street On-Ramp to Route I-195 WB) have been requested by MassDOT. The DWW-SPS JV has informed MassDOT that the additional engineering and permitting efforts associated with this scope change are in the order of \$65,000. These changes required an update to the noise study and resubmission of the Categorical Exclusion checklist. The project does still meet the requirements of a Categorical Exclusion from the Federal Highway Administration (FHWA).
- The DWW-SPS JV issued two Notices of Potential Costs and Delays to MassDOT on November 25, 2025. Both of these notices related to the City of New Bedford. The first notice relates to unresolved comments from the City of New Bedford on the 75% Design package, requesting resolution to a detailed list of outstanding items by 12/12/2025. The second notice states the need for continued coordination with the City of New Bedford as their adjacent combined sewer overflows separation project moves from design into construction, including the request for a formal coordination plan to ensure any changes to their signed and sealed contract plans are properly coordinated.
- The DWW-SPS JV is concerned that Verizon's work to move their fiber optic and copper lines from overhead to underground along Purchase Street and County Street is identified as the longest duration on the Project Utility Coordination Form (PUC Form) at 419 days and has not yet commenced. The DWW-SPS JV continues to attempt to coordinate with Verizon despite numerous fruitless attempts to date.

- An area of contaminated soil within the state highway layout has been discovered behind the property at 37 Washburn Street. The property appears to recycle tires and other automobile parts. The extent of contamination is not yet known as soil testing is ongoing, but the extent and levels of contamination appear to be significant. It appears that remediation work will be necessary by the DWW-SPS JV. This work is anticipated to be paid for utilizing contract pay items that were included in the contract for contingency purposes.

No other extra work, delays or costs have been discussed to date.

Progress Photographs



I-195 EB Lane Shift and Median Removal Progress – **August 2025**

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation
Owner's Representative Contract Number: 120633
Project Number: 606527
Construction Contract Number: 127763



Clearing & Grubbing along I-195 WB and Cedar Grove Street – **August 2025**



I-195 Median Removal Progress – **September 2025**

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation
Owner's Representative Contract Number: 120633
Project Number: 606527
Construction Contract Number: 127763



Ramp C (I-195 WB to Route 18 SB) Demolition Progress – **September 2025**



I-195 Median Removal Progress – **October 2025**



Area of Contaminated Soil Adjacent to 37 Washburn Street – **October 2025**



Aerial Location of Area of Contaminated Soil Adjacent to 37 Washburn Street

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation
Owner's Representative Contract Number: 120633
Project Number: 606527
Construction Contract Number: 127763



Ramp C (I-195 WB to Route 18 SB) Demolition Complete – **October 2025**



Work Area Secured Below I-195 Viaduct along Cedar Grove Street – **October 2025**

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation
Owner's Representative Contract Number: 120633
Project Number: 606527
Construction Contract Number: 127763



Shielding Installed Over Critical Sidewalks, Roadway, and Railroad Areas – **October 2025**



I-195 Traffic Shifted to Phase 2 Pattern – **November 2025**

Project Name: New Bedford – Route I-195 to Route 18 Interchange Rehabilitation
Owner's Representative Contract Number: 120633
Project Number: 606527
Construction Contract Number: 127763



Ramp C Infield and Viaduct Construction Access – November 2025



Ramp D (Route 18 NB to I-195 WB) Closed – November 2025

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF INTEREST

Peer Review

A peer review was not performed on this project prior to procurement.

Value Engineering

A Value Engineering (VE) Study was performed on this project several years prior to the decision to procure the project through the Best Value Design-Build procurement process. None of the recommendations from the VE Study were adopted at the time and the overall project design and scope has changed significantly since the VE Study was conducted. Note that MassDOT typically does not perform value engineering on design-build projects since it is inherently built into the process.

Cost Recovery

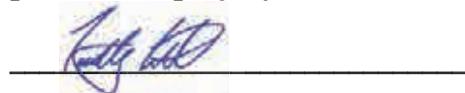
No issues have been specifically identified as Cost Recovery issues to date.

Extra Work Orders

As of November 30, 2025, there are no extra work orders that have been submitted. Potential extra work orders have been previously discussed herein.

OWNER'S REPRESENTATIVE OATH

I, Timothy Letton, MA P.E. No. 53308, hereby certify that my sole responsibility as Owner's Representative, under O.R. Contract 120633, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor, and subcontractors, and that this annual report is true and accurate to the best of my knowledge, signed under pains and penalties of perjury.



Timothy Letton, P.E.

Date: December 15, 2025



Maura Healey, Governor
Kimberley Driscoll, Lieutenant Governor
Phillip Eng, Interim MassDOT Secretary
Jonathan L. Gulliver, Undersecretary and Highway Administrator



Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension over MBTA and CSX Railroad and Merrimack River

Owner's Representative Contract Number: 120636

Project Number: 607887

Construction Contract Number: 128114



Owner's Representative's 2025 Annual Report

Owner's Representative Contract No.: 120636 Millennium Engineering Inc.

Project No. 607887: Lowell Rourke Bridge Replacement over MBTA, CSX and the Merrimack River

Preliminary Designer: HNTB

Prepared by: Millennium Engineering, Inc.

December 16, 2025

Ten Park Plaza, Suite 4160, Boston, MA 02116

Tel: 857-368-4636, TTY: 857-368-0655

www.mass.gov/massdot

Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

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Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

I. EXECUTIVE SUMMARY

The Rourke Bridge is a temporary 9-span bridge over the Merrimack River and the MBTA/CSX Railroad. MassDOT proposes replacing the temporary bridge with a permanent one. The Rourke Bridge replacement project is expected to reduce congestion over the bridge, and improve access for bikes and pedestrians, providing a multi-modal link across the Merrimack River. As part of the project, the intersections at either end of the bridge will be reconstructed to accommodate the new bridge. Improvements are also proposed to the surrounding roadways providing access to the bridge, including Wood Street, Middlesex Street, Pawtucket Boulevard (Route 113), and Old Ferry Road. The improvements include geometric changes to accommodate multi-modal travel as well as signal timing improvements. The purpose of the project is to improve traffic operations, safety, and accessibility on the Rourke Bridge and its approach intersections, for all roadway users, including vehicles, pedestrians and bicyclists.

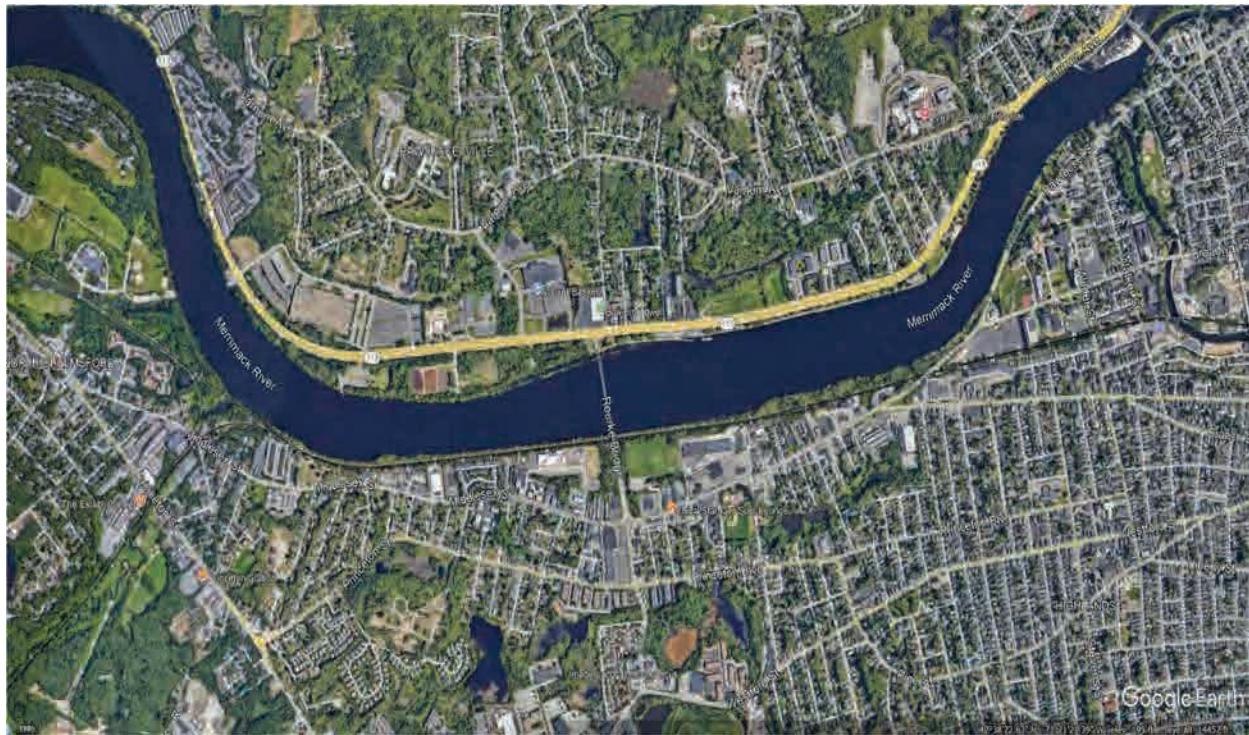
The project was procured as a Design-Build Contract Mass letters of Interest in April 2024, and Statement of Qualifications were received in July. Subsequently three firms were shortlisted on August 7th, 2024, and issued the request for Proposals on August 14th. Price and Technical proposals were submitted on December 20th with price proposals opened on January 23rd, 2025, with the Best Value being the Design-Build team of Skanska USA Civil Northeast, Inc.

Due to issues in obtaining the Right of Way certificate, the Notice to Proceed was issued in three steps. The first provisional Notice to Proceed was issued on May 15th, the second on August 19th, and the Final NTP was issued on September 9th.

The project is currently on schedule and within the budget amount, inclusive of contingencies.

Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

Figure 1 – Project Location



II. ANNUAL REPORT

Contract Scope of Work:

The Rourke Bridge (MassDOT Bridge No. L-15-088) is a temporary 9-span bridge over the Merrimack River and MBTA/CSX Railroad. The Massachusetts Department of Transportation is proposing the replacement of the temporary bridge with a permanent bridge. The Project is in MassDOT District 4 (Middlesex County), and the Project area includes the bridge itself, two major intersections, and their roadway approaches. In the north-south direction, the limits of the Project extend about 400 feet along Wood Street from the Rourke Bridge to about 800 feet along Old Ferry Road from Pawtucket Boulevard. On the southern side of the Merrimack River, the limits of the Project extend along Middlesex Street about 550 feet to the west of the Wood Street intersection and about 450 feet to the east of the Wood Street intersection. On the northern side of Merrimack River, the Project limits extend along Pawtucket Boulevard about 950 feet west of the Old Ferry Road intersection and about 600 feet east of the northern end of the existing Rourke Bridge.

Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

The northbound approach of the proposed permanent bridge will be located at the intersection of Wood Street and Middlesex Street in the same location as the northbound approach of the existing Rourke Bridge. However, the permanent bridge will curve to the west as it crosses the Merrimack River, and its southbound approach will be located at the intersection of Pawtucket Boulevard and a realigned Old Ferry Road instead of near Townsend Avenue like the existing bridge. The intent of the Project is to replace the temporary Rourke Bridge with a permanent bridge that meets multimodal and ADA standards and improves mobility between the north and south sides of the Merrimack River for residents, businesses, and visitors.

The roadway work at the intersection of Wood Street and Middlesex Street is comprised of excavation, mill and overlay, replaced sidewalks and wheelchair ramps, pavement markings, new traffic signal and equipment, and other incidental items. The roadway work at the intersection of Pawtucket Boulevard and Old Ferry Road is comprised of excavation, mill and overlay, new shared use paths, pavement markings, new and replaced wheelchair ramps, new traffic signal and equipment, and crosswalks. Roadway work at this location also includes an alignment shift of Old Ferry Road to align it with the northern end of the bridge.

Major Progress as of November 30, 2025

During 2025 Skanska USA Northeast LLC, made numerous design submittals in support of the proposed construction activities, the most significant are: early release highway alignments; stormwater pollution prevention plan; geotechnical report for early release for construction foundations; early release for construction foundations-drilled shafts-piles; track monitoring plan; stormwater management plan; 75% Highway Design; geotechnical report highway elements; temporary cofferdam design; and first structural submittal and scour report; and Monthly Schedule Updates #1-5

In addition, the following construction work has taken place:

- Installing erosion control devices at the north abutment area.
- Setting temporary barrier on Pawtucket Boulevard eastbound to close the right hand shoulder

Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
 Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

Budget:

- Office Estimate: \$206,789,564
- Price Proposal: \$273,840,000
- Encumbered Amount: \$299,506,400
- Total Amount Expended through November 30, 2025: \$2,653,577
- Current Estimate at Completion: \$276,106,400* Includes Municipal Participation
- The table below contains a summary of project financials through November 30, 2025

Project Budget / Financials			
	Contract	MassDOT	Municipal
Bid	\$ 273,840,000	\$ 270,240,000	\$ 3,600,000
Allowances	\$ 2,266,400	\$ 2,266,400	\$ 0
Original Contract Value	\$ 276,106,400	\$ 272,506,400	\$ 3,600,000
Original Contingency (FIN681)		\$ 27,033,160	
Original Encumbrance		\$ 299,539,560	
Encumbrance Modifications		\$ 0	
Current Encumbrance		\$ 299,539,560	
Current Overruns and Underruns	\$ 0	\$ 0	\$ 0
Approved Contract Modifications	\$ 0	\$ 0	\$ 0
Current Contract Value	\$ 276,106,400	\$ 272,506,400	\$ 3,600,000
Pending Contract Modifications		\$ 0	\$ 0
Estimated Contract Total	\$ 276,106,400	\$ 272,506,400	\$ 3,600,000
Probable Contract Modifications		\$ 0	
Forecast Cost-at-Completion	\$ 276,106,400	\$ 272,506,400	\$ 3,600,000
Total CQE (Invoiced) to Date	\$ 2,653,577	\$ 2,653,577	\$ 0
Funds Remaining		\$ 296,885,983	
Projected Funds Remaining		\$ 27,033,160	
Expected Progress (baseline)	0.00%		
% Complete	0.96%		

Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

Schedule:

- Contract Award: May 1, 2025
- Notice to Proceed (NTP) Date: September 9, 2025
- Substantial Completion: September 11, 2029
- Final Completion: April 15, 2030

Areas and Issues of Concern

- It is imperative that Skanska USA Northeast LLC work through MassDOT's comments on the structural design for the various bridge components. While the design is moving forward, it seems as though the comments are being addressed in a piecemeal fashion, not in a comprehensive manner.
- Skanska USA Northeast LLC and MassDOT are working through the permitting requirements for dewatering of the cofferdams and the remaining tanks from the old City of Lowell wastewater treatment facility. If the required permits aren't obtained in a timely fashion then there could be an impact to the project schedule.

III. CHAPTER 303 OF THE ACTS OF 2008 AREAS OF EMPHASIS

Peer Review

During the preliminary design phase of this project, a peer review was performed by MassDOT's Highway Division forces independent of the project designer. In addition, Gill Engineering performed a peer review of the structural elements of the proposed structure. These reviews were performed to identify conflicts or errors that should be corrected or mitigated at each stage before design progressed further. This Owner's Representative was not involved during the preliminary design phase but has determined that the peer review was performed through discussions with MassDOT personnel.

Value Engineering

There was no specific Value Engineering study performed; however, the Design-Build procurement method is considered a Value Engineering technique.

Project Name: Rourke Bridge Replacement, L-15-088, Wood Street Extension
Over the MBTA and CSX Railroad and Merrimack River
Owner's Representative Contract Number: 120636
Project Number: 607887
Construction Contract Number: 128114

Cost Recovery

At this point in time, no issues have been specifically identified as Cost Recovery issues. However, it should be noted that any initial identification of a Cost Recovery issue does not necessarily mean that cost recovery is due; a formal evaluation process must still be followed. The undersigned Owner's representative will continue to monitor issues and, with MassDOT Highway Division, will discuss the need for its referral to the Cost Recovery Standing Committee. For any issue referred for Cost recovery, the current MassDOT Highway Division Cost Recovery Procedure (SOP No. HED-70-01-1-000, dated 9/25/06) shall govern. To fulfil the legislative mandate of being the Primary manager of cost recovery, the Owner's representative will follow the referenced SOP. This SOP, as modified for projects with an owner's representative, describes the role of the Owner's representative and includes a Cost Recovery Standing Committee, which oversees the work of separate and issue-specific Cost Recovery Review Panels and Cost Recovery Evaluation Committees, which will be convened as necessary. It should also be noted that the resolution of cost recovery issues typically does not start until the end of a project, to ensure working relationships are maintained and insurance notifications occur only once a final decision has been made. In addition, in moving forward with future resolution of Cost recovery issues, it is recommended that the resolution follow the path set forth in the recently developed Draft MassDOT Owner's Representative Standard Operating Procedures.

Oath

I, Eric W. Botterman, P.E., MA P.E. No. 41099, hereby certify that my sole responsibility as Owner's Representative, under Owner's Representative Contract 120636, is to the Commonwealth of Massachusetts and MassDOT. Furthermore, I certify that I act in a manner that is wholly independent of the project's designer, general contractor and subcontractors, and that this annual report is true and accurate to the best of my knowledge signed under the pains and penalties of perjury.



Eric W. Botterman, P.E.

Date: December 16, 2025